

# PLAN AND ACTION PROGRAM

Prepared for the  
City of Boise, Idaho

## CAPITOL BOULEVARD PLAN AND ACTION PROGRAM

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PLAN AND DEVELOPMENT UPDATES

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## INTRODUCTION

This report details a plan and action program for the revitalization of Capitol Boulevard as Boise's ceremonial entrance. The distinctive character of Capitol Boulevard comes from its strong visual axis that focuses on the imposing Union Pacific Depot and the state Capitol with its dome silhouetted against Boise's foothills.

Originally opened in 1931, the one and one-quarter mile boulevard ties together the city's best assets. From the statehouse through the downtown it connects the historic districts of Old Boise and the 8th Street warehouse area and includes Julia Davis Park with its zoo, historic museum, art museum, and adjacent library, Boise River Greenbelt, Boise State University, a motel and restaurant area, and, anchoring on the south, the Union Pacific Depot. These special resources represent strong centers of government, commerce, finance, entertainment, culture, education, and transportation. Although currently lacking visual coherence the boulevard connects these growing centers with a unique sense of spaciousness.

Responding to the need to help fulfill the original intent of a grand boulevard, the American Institute of Architect's 1985 Boise R/UDAT team recommended that the community prepare a plan and adopt guidelines to transform Capitol Boulevard into a place of grandeur. Such efforts have been fostered by the Idaho Chapter of the American Institute of Architects, who, through a sub-committee, analyzed the boulevard and developed this beautification plan that

is responsive to the boulevard's ceremonial character and the opportunity to refine and improve its many components. The existence of this plan is the product of the commitment of the City of Boise and many supportive people, who contributed to the enterprise of planning Capitol Boulevard.

The plan provides an illustrative pattern of the boulevard's history, defines major public improvements, and establishes guidelines by which all future proposals for modifications can be measured. Capitol Boulevard has the opportunity to grow as an active and vibrant center, linking numerous opportunities and adventures. It can serve as the city's focal point where Boise's citizens can congregate not simply for special occasions but for the daily urban pleasures of recreation and work in agreeable settings.

The intent of this plan is to revitalize the boulevard and encourage a return to the scenic, landscaped boulevard that was originally envisioned. This plan sets into motion the public and private support that will see the boulevard's vision fulfilled.

## THE STUDY AREA

This plan focuses on the Capitol Boulevard corridor, an area of approximately 20 blocks in length. The long, narrow study area extends from the statehouse on the north to the depot's railroad tracks on the south, with a width of at least one block on each side of the boulevard's right of way. Figure 2

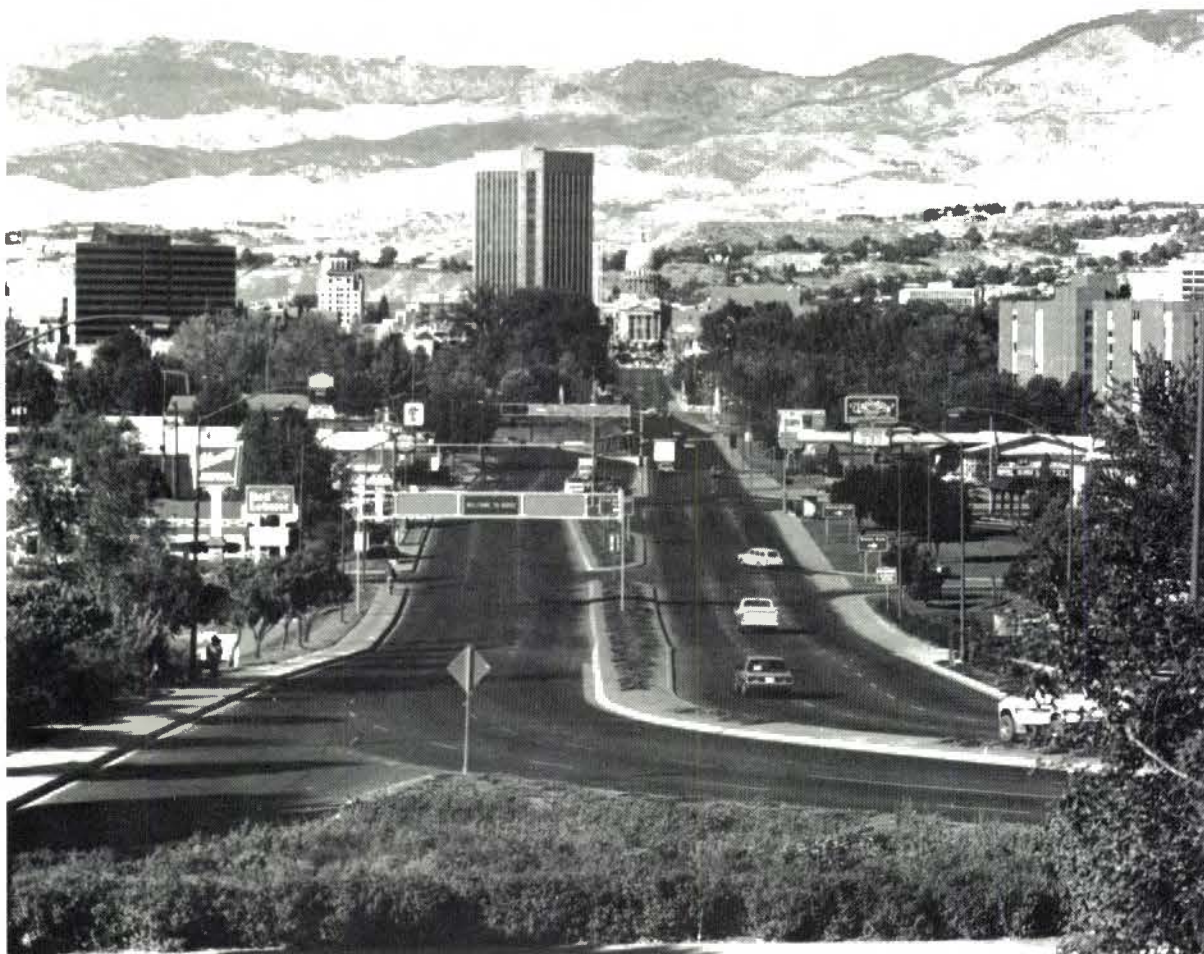


Figure 1. Looking north on Capitol Boulevard.

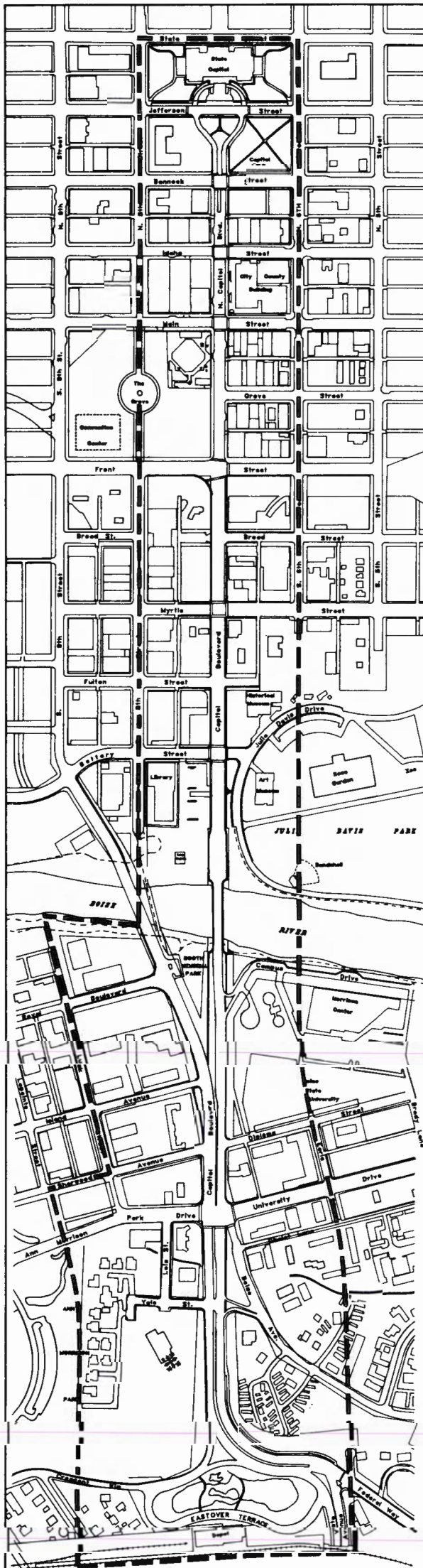


Figure 2. Study Area and Capitol Boulevard Special District.

illustrates the study area boundaries. The one and one-quarter mile boulevard possesses a uniform street and block grid north of the river; however, south of the river the alignment of streets is altered at the crossing of South 9th Street and Boise Avenue where it forms more north-south block patterns and larger odd size lots.

The width of the boulevard's right of way north of the river is 80 feet, expanding for a short 100-foot wide segment near the Capitol Memorial Bridge. The blocks within the Original Townsite and the Davis Addition facing the boulevard have a frontage of 260 feet and a depth of 300 feet. Those few blocks facing Capitol Boulevard south of the river have a 250-foot frontage.

## METHODOLOGY

This plan has resulted from a comprehensive study undertaken by the Idaho Chapter of the American Institute of Architects. Initially started by a small study committee of architects, landscape architects, and planners in late 1985, the project was granted funds by the City of Boise in January of 1988. The steering committee, which was expanded, is acknowledged in the credits.

Looking at the residents of Boise and Idaho as the clients, the plan for Capitol Boulevard was undertaken as a citizen's project. A questionnaire on the future of Capitol Boulevard was solicited from the state's architects and published in the *Idaho Statesman* to help secure a broad base of ideas for improving the boulevard. Additional information was collected from inventories of historic buildings, trees, and street lights, numerous interviews with key agencies, property owners, and managers, a walking tour, mapping of the changing physical elements, and monthly steering committee meetings. Documentary photographs from the Idaho Historical Society disclosed much about the development and appearance of the early boulevard. A review of comparative studies and pertinent literature was conducted and is listed in the bibliography. By studying the boulevard's history and its visual characteristics and reviewing suggestions obtained throughout the planning process, the plan was formulated.

Although the boulevard cannot be literally restored to its 1931 opening, this plan does interpret the original intent of the boulevard's early planners while satisfying the contemporary demands of circulation. The results of this effort is the plan detailed in these pages.

## GOALS AND OBJECTIVES

The plan is intended to provide for the development of Capitol Boulevard as the major entrance to our Capitol City and as the symbolic ceremonial connection between the Capitol and the Union Pacific Depot. The following objectives formed the basis for planning Capitol Boulevard:

- A. To transform Capitol Boulevard into a true "Boulevard," a "broad city avenue having a park-like appearance, with areas at the side or center for trees, shrubs, lawn, and flowers";
- B. To enhance the appearance of Capitol Boulevard to motorists entering the Capitol City as well as to encourage pedestrian and bicycle use of the boulevard by improving comfort, safety, and security;
- C. To establish a system of identification for places of civic interest adjacent to Capitol Boulevard, including the Boise River Greenbelt, art and historic museums, library, Boise State University campus, Morrison Center, Eighth Street Marketplace, Old Boise, and the downtown;
- D. To strengthen the visual tie between the two main historical buildings on Capitol Boulevard; namely, the Capitol and Union Pacific Railroad Depot; and
- E. To provide a mixture of commercial and cultural activities along the boulevard that will attract a wide variety of people, transforming the boulevard into an attractive and pleasant place for residents and visitors alike.

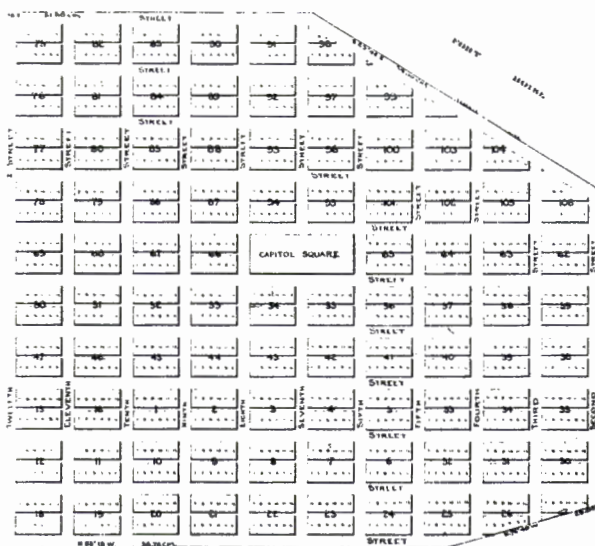


Figure 3. The 1867 plat of the Boise City Original Townsite reserved a double block centered on 7th Street for Capitol Square.



Figure 4. 1909 drawing of Idaho's State Capitol by architects Tourtellotte and Hummel.

## HISTORY

The origin of Capitol Boulevard can be traced to the establishment of the grid platting of Boise City's 1867 Original Townsite. Within this plan, two blocks bounded by State, Jefferson, 6th, and 8th streets were titled "Capitol Square" and set aside for the Capitol. When the territorial Capitol was built on this space in 1886 little concern was shown for classical planning. The three-story brick and stone structure was placed on the eastern half of the square rather than at the center. Similarly, Central School had been built on the western end of the square a few years earlier.

The opportunity to provide a view of the Capitol from 7th Street—later renamed Capitol Boulevard—came in 1905 when architects Tourtellotte and Hummel developed plans to enlarge the Capitol. The design of Idaho's Capitol and its approach were influenced by both national and European traditions. The Chicago Great White City Exposition of 1893 had a far-reaching effect on the appearance of American cities. Its influence soon extended to Boise, subtly altering the city's plan in favor of a neo-classical approach to space that coordinated various elements into a unified whole. The nation's Capitol, on which Idaho is based, had been provided with a formal European-style setting in Major Pierre L'Enfant's 1792 plan for Washington, D.C. Long radiating vistas extended from the Capitol and the White House, with open circles and squares at a number of major intersections. Frenchman L'Enfant, who had served under Washington during the Revolution, naturally followed the Baroque spatial concepts popular in Europe since the late 16th century. Symmetrical balance and sweeping vistas were the rule in the planning of Versailles, Paris, Vienna, and Rome. In his presentations, Capitol architect J. E. Tourtellotte often dwelt upon the need to provide grand approaches to the Capitol and assure the axial siting for maximum visual impact.

With Idaho's Capitol dome and portico centered on 7th Street, the American City Beautiful Movement echoed the new taste for formal landscape planning, which further encouraged the enhancement of Boise. Architect Charles Wayland, president of the Boise Civic Improvement Association, wrote an article for the January 1, 1914 Idaho Statesman entitled "Plan to

Beautiful Boise and Control River Without Cost.” The drawing submitted with the article shows the Capitol as the civic center, with Seventh Street broadened from 80 to 180 feet and with medians and parks stretching along the street to the river. It also showed parkways running along the river. Though the street was never widened, the sketch helped to further the boulevard concept and the classically designed bridge was built as illustrated, completing the boulevard in 1931.

Capitol Boulevard was fully realized when the Union Pacific Railroad Depot was built on the bench to the south in 1925. This siting on an axial alignment one and one-quarter miles from the Capitol created a dramatic approach to the city. New York architects Carrère and Hastings, who designed the depot, were

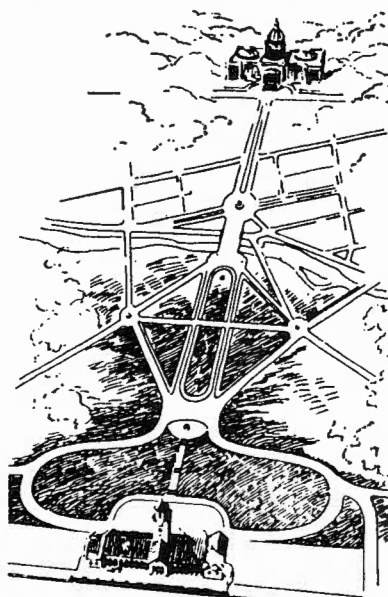


Figure 6. 1924 plan for Capitol Boulevard.

French-trained proponents of neo-classicism. Their 1924 plan for Capitol Boulevard called for a broad vista with medians and extending radial arms. Though simplified, the depot hill approach was built reflecting this plan.

The opening of the Capitol Boulevard Memorial Bridge and subsequent landscaping along portions of the boulevard met with great approval. News articles at the time stated that “the bridge and the boulevard form an approach to the city which is unequalled in its stark beauty and simple grandeur by any bit of man-made highway in the west. To the north of the bridge, a road cuts through Boise, leading directly toward the gleaming brass doors of the state capitol building. To the south, a bank of green and color lies at the turning of the highway, while the depot pierces the skyline. Motorists entering Boise immediately will be struck by the panoramic view of the city before them. It will be different than anything their wandering previously had shown. The visitor will be inclined to long remember the Idaho Capitol because of the first glimpse he had of the city.”

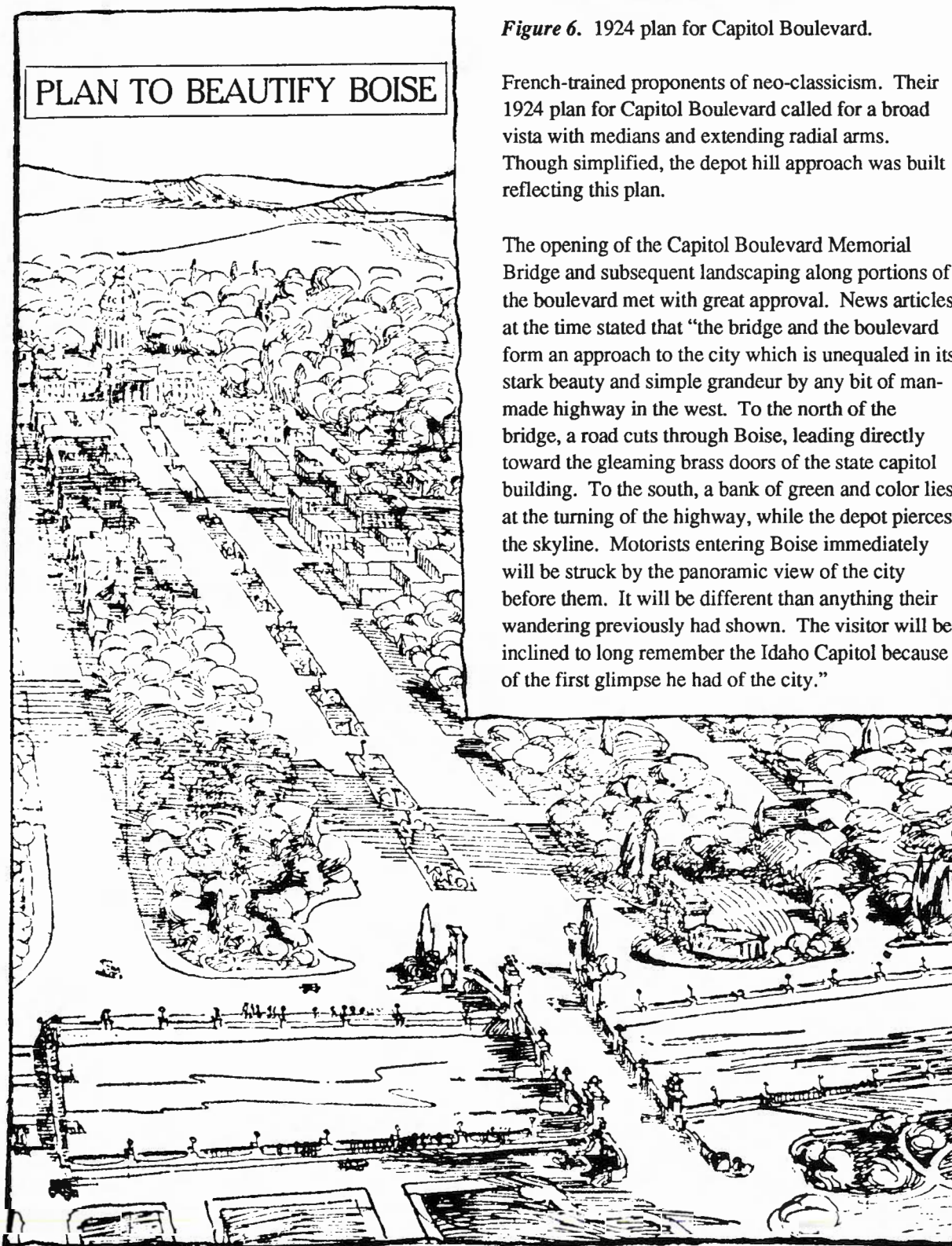


Figure 5. Sketch presented by Boise Civic Improvement Association for January 1, 1914 Idaho Statesman story.

## IDAHO STATE CAPITOL

By 1906 Idaho had set its sights on constructing a modern state Capitol. Ground was broken that year and the central or dome portion of the building was completed by 1912 and the wings in 1921. Designed by architects Tourtellotte and Hummel, the Capitol stands 208 feet high from street level to the top of the winged eagle liberty. Some 398 feet long by 224 feet in depth the Capitol, when constructed, contained more marble than any other building of its size in the country. Much of the building's superstructure is faced with Boise sandstone quarried from nearby Tablerock.

The Idaho State Capitol is patterned after the nation's Capitol in Washington D.C., wherein a unifying dome between balanced wings expresses a bicameral legislative system. Its classical ornamentation expresses the American notion of Roman republican virtues. A preeminent example of neo-classical architecture, the dome of the building is considered one of the most perfect of all state capitols. The dome is supported on a grand colonnade of beautiful Corinthian columns sixty feet in height, forming a splendid interior rotunda space. One regional expression of the Capitol is evident in the rustication of the basement level, where the sandstone can be read in an Idaho context as stacked and stylized logs.

In 1919 the legislature, not quite comfortable in the central building, authorized construction of the east and west wings. The east wing was built upon the former site of Idaho's territorial Capitol, while the west wing was built upon the former site of the

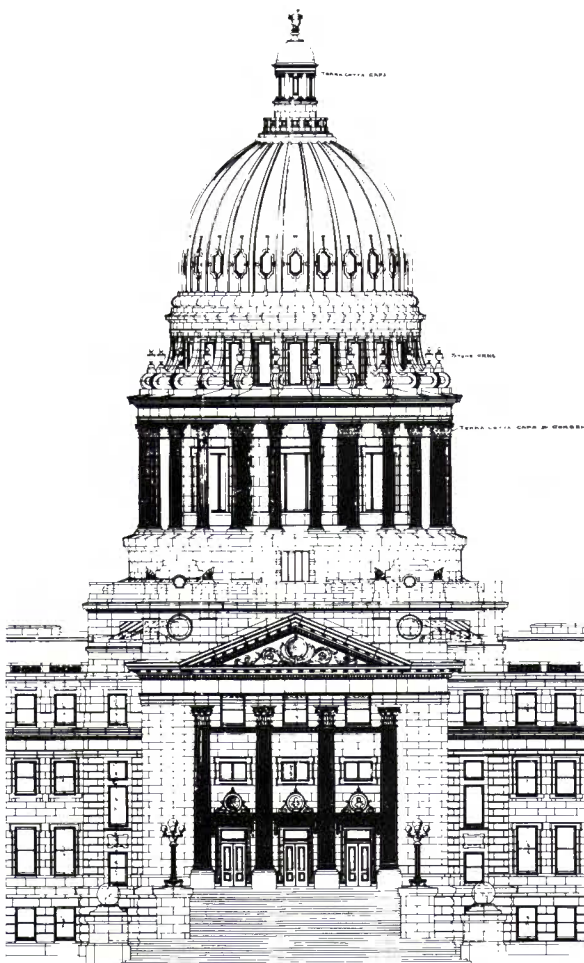


Figure 7. Capitol drawing by J.E. Tourtellotte.

Central School building. The house and senate have circular chambers with vaulted shaped ceilings, and the interior of the building is noted for its light and open spaces.

An edifice of splendor and grand proportions, the Capitol is beautifully scaled. South facing so that it catches the light on its strongly sculptured surface the building is sited at the end of the boulevard where no one could miss it with its back to the hills. In its setting Idaho's Capitol is as impressive as any in the nation.



Figure 8. Chinese New Year's Day Parade showing the Capitol under construction. c.1910. Constructed in 1879, the Perrault Building, located on the right, is the boulevard's oldest building.

## CAPITOL GROUNDS

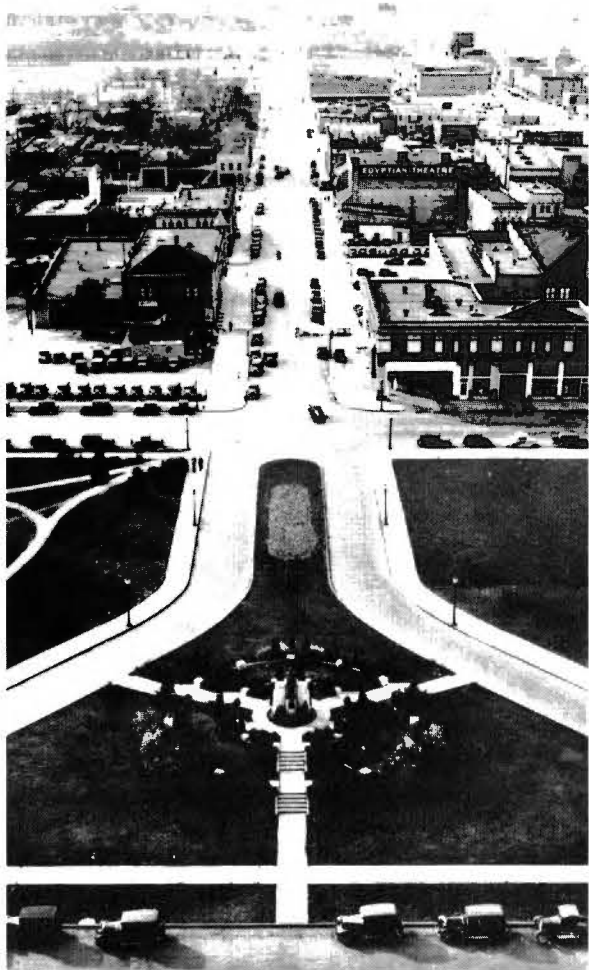
Landscaping of the Capitol grounds has been an ongoing process since construction of Idaho's territorial capitol. One of the most noteworthy features is the "presidential grove," with trees planted by Benjamin Harris in 1891, Theodore Roosevelt in 1903, and William Taft in 1911. Also, many trees on the grounds have been planted either to commemorate or honor governors and other individuals or to serve as a memorial. Other trees mark national or state celebrations. Idaho's state tree, the western white pine, is also planted on the grounds.

In 1930, comprehensive plans for the landscaping of Capitol Park, the Capitol grounds, and the courthouse grounds were drawn up by John A. Jensen and E. R. Bennett. Governor H. C. Baldrige felt there should be more unity in the treatment of the three parks and started the movement to obtain it.

Landscaping at the statehouse was again undertaken in 1954 when old shrubbery was removed to make way for roses, quince, flowering almond, and other numerous varieties of flowers, evergreens, and shrubs. In recent years many of the elms have been removed and numerous young trees have been planted. Also, new, gracefully curved walks were installed on the east side of the statehouse from both Jefferson and Sixth streets to the statehouse steps. A number of memorials grace the Capitol grounds and Capitol Boulevard and are discussed on page 11.

## CAPITOL PARK

The dignity and grandeur of the new Capitol called for a spacious approach, both out of respect to the physical proportions of the structure and for the statehood it represents. Over the years, the City of Boise has taken a number of steps to afford an appropriate setting for the Capitol. When the legislature made appropriations for completion of the Capitol in 1919, it was under the conditions that the City of Boise acquire the two blocks of land opposite the Capitol grounds on Jefferson Street, remove the buildings, develop a park area, and maintain the grounds in perpetuity for park purposes. A bond election was passed to purchase the land. The Collister Apartments, located on the southwest corner across from the Capitol, blotted out the view to a certain extent and, thus, were razed. In the early 1920's, the 7th Street approach was made into a boulevard entrance with a triangular central island, and the remaining blocks were converted to park land. For a time the park was referred to as Government Place. In 1929 a memorial to Governor Steunenberg was erected directly across from the Capitol's main entrance on the triangular site. A Statesman article, dated May 3, 1931, mentions that the "Statehouse approach offered wayfarers a shaded retreat." The article tells of a triangular block of transplanted paradise located in front of the statehouse, along with 16 green benches, trees, paths, and a wide expanse of verdant lawn. It concludes, "All this is Capitol Park—an oasis in the midst of scorching pavement, honking horns and the buzz of daily life in a busy city." In the 1960's, proposals were



**Figure 9.** View from the Capitol dome with Capitol Park in foreground. c. 1932.

made to build a new federal building and later a parking lot in Capitol Park but wisdom prevailed and the grassy tree-covered expanse continues to enhance the dignified beauty of the Capitol.



**Figure 10.** Oregon Short Line Depot.

## THE DEPOT

Due to its impressive architecture and landscaping, Boise's Union Pacific Depot offers a striking first impression. Construction of the depot was begun in 1924 and on April 16, 1925, Boise and southern Idaho joined in celebrating the arrival of the first train over the new main line, which pulled into what many considered to be the finest station in the Northwest. Spanish colonial in style, with its white stucco finish, red tile roofs, and commanding bell tower, Boise's depot presents a welcomed vista to both visitors and residents.

The depot was designed by R.H. Shreve and Thomas Hastings of the New York City architectural firm of Carrère, Hastings, Shreve & Lando. The depot is trimmed in Boise sandstone and features a fine reception room. Six arched entrances and arched upper story windows allow a flood of light to enter the waiting room with its high ceiling. Spanish mission in style, this large room has a trussed ceiling trimmed with an Indian design and is appointed with tile floors and oak benches. The ticket office window graces one end of the room with the emblem of the Union Pacific system over the window arch. The clock tower chimes were added in 1927 in honor of Edward Henry Harriman. (See page 13.) The bells were supplied by the Meneely Bell Company of New York and Seth Thomas Company supplied the clock and striking device. The chimes would ring with their Westminster peal every quarter hour. The depot is surrounded by a paved brick platform extending a block in each direction along the tracks. Fronting the depot is a commanding view of the city and ample room for taxis, buses, and parking. Single-globe decorative cast iron lights grace Eastover Terrace.

For those departing on Amtrak the view from the rim plateau can hardly be surpassed, offering a sweeping vista of the Capitol dome, downtown, parks, Boise State University and the imposing foothills. Likewise, the depot serves as a landmark to the city, penetrating the Capitol's skyline.



Figure 11. Platt Gardens. c.1930.

## PLATT GARDENS

The depot approach park, known as the Platt Gardens, was designed by a noted Spanish landscape architect, Ricardo Espino. Built by Union Pacific in 1927, the depot gardens feature a winding walk, benches, ponds, a monument of volcanic rock, and a welcoming display of greenery. Handsome mission style lights on sheppard crook poles grace the garden walks. The site's bench vantage point provides an impressive view of the city. The depot and gardens greet visitors with one of the most attractive city entryways. The garden was named for Howard V. Platt, who at the time was general manager of the original Oregon Short Line Railroad.

To assist the development of the depot road approach, city property owners approved a \$125,000 bond issue on June 27, 1924. Bond issue opponents at the time stated that "the depot was located too far from the city."

DOES BOISE WANT THIS KIND OF AN APPROACH?



*Surely Not.-Then Vote **FOR** the Bond Issue!*

Figure 12. Cartoon in support of depot approach bond issue. Published on June 26, 1924 in the *Idaho Statesman*.

## CAPITOL BOULEVARD MEMORIAL BRIDGE

In 1931, Capitol Boulevard was officially opened with a ceremony for the Capitol Boulevard Memorial Bridge, dedicated to the pioneers of the Old Oregon Trail. Forming an approach to the Capitol and depot, the bridge's four graceful arches span the Boise River. The bridge's glistening white concrete sides reflect the sun's rays and the graceful handrails and decorative pylons are a tribute to the structure's craftsmanship.



Figure 13. Capitol Boulevard Memorial Bridge opened in 1931.

The bridge was located on the Old Oregon Trail Highway near the city's center. Intended to be of some prominence, the design by Charles H. Kyle, State Bridge Engineer, called for a graceful concrete structure with decorative pylons at each end embedded with bronze plaques and mosaics paying tribute to the pioneers who use to ford the Boise River at that point. The bridge also incorporated cast concrete light standards and a curved stairway leading down to what is now the Julia Davis Park greenbelt.

The handsome bridge was erected by Morrison-Knudsen Company in 1931. One-hundred workers labored on the structure, working 16 hours a day, 7 days a week. The construction crews took pride in their craftsmanship with each member feeling a certain satisfaction in having taken part in building Idaho's monument to the pioneers. The 301-foot bridge, which took six months to build—two months ahead of schedule,—consists of four 70-foot open spandrel arches. The roadway provides a 40-foot width with a 6-foot wide sidewalk on each side. The original decorative features are virtually intact.

Boise's new bridge was opened with a colorful and eventful celebration. The city's residents set aside the day to make the event one of tribute to those who had traveled the old trail. Businesses, as well as city, county, and state buildings, closed for part of the day. Mayor J. Pope gave the new concrete arch to the public on September 15, 1931 with the ceremony taking place at the bandshell in Julia Davis Park. It was preceded by a historical pageant parade, municipal band concert, and the community singing "Idaho," "America the Beautiful," and "America." Events taking place after the ceremony included a baseball game and dancing.

A National Park Service inventory states that "the Capitol Boulevard Bridge is of exceptional significance as one of the finest examples of concrete bridge architecture in the State of Idaho. The structure is

outstanding for its beautiful and unusual artistic features. Its prominent location as an approach to the City of Boise further emphasizes its symbolic significance." The greenbelt was extended under the south side of the bridge in the early 1970's.

#### EIGHTH STREET BRIDGE

The nearby South Eighth Street Bridge was built in May 1911 by the Missouri Valley Bridge and Iron Works. It replaced an earlier bridge at this site, which in turn replaced the McClellan Ferry. The bridge consists of two 160-foot pin-connected steel Pratt through-truss spans for a total length of 322 feet. The roadway is 19 feet wide. The upper chords consist of laced channel sections with cover plates, lower chords of eyebars, verticals of laced channel sections, and diagonals of steel eyebars and eyebars with turnbuckles.

In 1979 the bridge was cleaned and painted and in 1988 the Ada County Highway District rebuilt the approaches. Usage was converted to bicycles and pedestrians.

According to the National Park Service, "The South 8th Street Bridge is highly significant as the sole remaining steel highway truss bridge in Boise and is one of the most substantial Pratt truss spans in the state. It is representative of typical methods of bridge construction during the city's early history. The truss stands in sharp contrast alongside the Capitol Boulevard Bridge, a multi-arch concrete structure representing the dramatic advances in bridge technology since the erection of the earlier bridge." Continuing evolvement of bridge design is shown in the new, less notable 9th Street Bridge.

#### TRANSPORTATION

Capitol Boulevard plays a major role in Boise's transportation system, connecting vehicular, air, and passenger train traffic. Boise's first airport was located off Capitol Boulevard where Boise State University now stands. Today, the airport, which is located four miles south of the depot, is directly tied to the city center via the boulevard and Vista Avenue. The Union Pacific Depot, prominently located on the rim overlooking the city, is Boise's anchor to passenger train service. The opening of the new depot in 1925 also led to road improvements to the depot hill approach in 1927, which connected Federal Way to Capitol Boulevard. This new roadway tied Old Oregon Trail Highway U.S. 30 to Capitol Boulevard, crossing the river on the 8th Street Bridge. With the opening of the Oregon Trail Memorial Bridge in 1931, Capitol Boulevard served as U.S. Highway 30, 20, and 26. As the region's major east-west highway, U.S. 30 routed all travelers through Boise and down Capitol Boulevard.

In the 1920's, automobiles began to influence the city's mode of travel. With advances in the private automobile making longer trips possible, auto travel became popular on the country's pioneer highway.



*Figure 14.* Grass median leading up to the depot. Notice cast iron lights. c.1930's.

This early auto travel led to a number of auto camps being established at the south end of Capitol Boulevard. These tourist courts were the predecessors to the boulevard's motels. This same auto travel also led to the opening of a number of service stations being built on Capitol Boulevard.

Boise's growth and increasing vehicular traffic has led to widening of the street. Various segments of the boulevard's road bed have been rebuilt and resurfaced over the years. One section was widened to 60 feet in 1951 and another project in 1958 widened a portion to 63 feet. In 1965 the Boise Avenue intersection was modified creating a small triangular park.

A significant change occurred in 1969 when Interstate 84 opened just north of the airport. This routing allowed travelers to by-pass Capitol Boulevard and the city center, resulting in less commercial activity

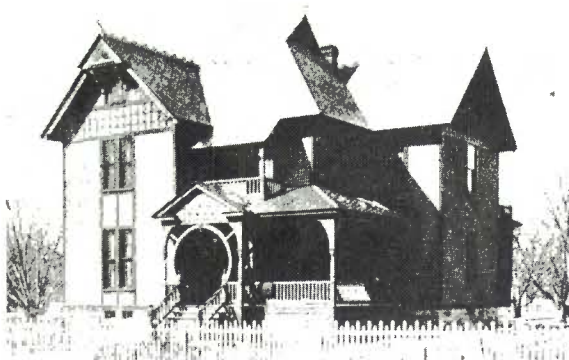


*Figure 15.* Capitol Boulevard--looking north from Platt Garden. c.1935.



**Figure 16.** 1938 aerial view of Capitol Boulevard.

for boulevard businesses. By 1974, other changes to Boise's arterial system led to the boulevard losing its designation as a federal highway and being reclassified from Federal Aid Primary to Federal Aid Urban. Capitol Boulevard was removed from the state system in 1979, converting to the authority of the Ada County Highway District. This same period also saw enlargement of the University Drive intersection. The depot approach was rebuilt and widened in 1985 with the expansion of Vista Avenue and Federal Way from two lanes to six lanes and the elimination of northbound traffic from Crescent Rim. Another significant change occurred with the construction of a new 9th Street Bridge in 1987 and the conversion of most of Capitol Boulevard to one-way northbound traffic. Most recently, four downtown blocks were reconstructed slightly narrowing the boulevard's roadway to 52 feet.



**Figure 17.** 1891 residence of Thomas Jefferson Davis, formerly located at 415 Capitol Boulevard.

## BOULEVARD DEVELOPMENT

In its early years, private development along the boulevard occurred primarily in the downtown section north of the former Barber Spur rail line. With the opening of the Capitol Bridge in 1931 and the advent of automobile travel, new businesses began to appear from Front Street to the University Drive intersection.

In keeping with the spirit of this ceremonial entrance the city of Boise under Mayor J.P. Pope established zoning provisions in 1932 regulating architecture, setbacks, and signing. Ordinance No. 1589 established architectural restrictions subject to the Board of Adjustment, as well as establishing a ten-foot building setback for the entire length of Capitol Boulevard south of Front Street. (See page 10.)

Signs were not to protrude beyond the 10-foot setback line along Capitol and they were to be illuminated. Another restriction was that billboards, bulletin boards, signboards, or advertising would not be erected along or within the lots facing Capitol Boulevard. One more restriction was that gasoline pumps or air lines would not be installed between the building lines of Capitol Boulevard. Unfortunately, many of the boulevard's signing and architectural restrictions were not followed; however, the ten-foot setback has been respected since that time. This building setback can be best appreciated in the



**Figure 18.** Governor C.A. Bottolfson and Thomas E. Dewey viewing boulevard in 1943.

boulevard's Front Street to Boise River section where a few buildings built in the 1930's and 1940's were built exactly ten feet from the property line. Uses in this area turned to automobile sales and service stations in the 1940's. The southern end of the boulevard developed with a concentration of early tourist camps, followed by motels along with a number of service stations and restaurants that catered to the travelers on Highway 30, the Old Oregon Trail.

Cultural development was fostered in Julia Davis Park when the Boise Art Gallery opened in 1932, facing the boulevard. An expanding cultural center has since prospered with the addition of the Idaho Historical Museum and Boise Public Library and the expansion of Boise City College into Boise State University.



**Figure 19.** 1972 view of Capitol Boulevard.

City of Boise, Idaho

# ZONING LAWS 1932

## ORDINANCE NO. 1589

### "C" COMMERCIAL ZONE

#### Capitol Boulevard Restrictions.

Section 12. Every building hereafter erected or structurally altered or repaired or remodeled on lots facing and for the entire length of Capitol boulevard in Zone C shall be subject to such architectural restrictions as may be required by the Board of Adjustment. Also, before a permit is issued for the erection or remodeling of any structure within this portion or part of "C" Commercial Zone, the applicant shall file with the Building Inspector such plans and specifications as may be required to show the design of such proposed building or alterations.

Gasoline pumps or air lines shall not be installed between the building lines of Capitol boulevard.

#### Street Signs.

Section 13. Electric signs shall not project more than four (4) feet and six (6) inches from the building lines along Capitol boulevard.

#### Roof Signs.

Section 14. Roof signs shall not project beyond the property line or set-back line along Capitol boulevard. They shall be illuminated and the design approved by the Board of Adjustment. Not more than fifty (50) per cent of such signs shall be solid area. Letter boxes to be fastened to skeleton frame-work and each letter to be illuminated as required of Class A, B, and C electric signs.

### "D" COMMERCIAL ZONE

#### Capitol Boulevard.

Also all that land lying within 200 feet of the side lines of Capitol boulevard and Ninth street pike from Boise river south.

#### Capitol Boulevard Restrictions.

Billboards, bulletin boards, signboards or advertising signs shall not be erected along or within the lots facing Capitol boulevard.

Gasoline pumps or air lines shall not be installed between the building lines of Capitol boulevard.

#### Capitol Boulevard Set-Backs

Section 16. In that portion of "D" Commercial Zone lying between the alley between Capitol boulevard and Eighth street and Capitol boulevard and Sixth street and extending the entire length of Capitol boulevard south of Front street, every building hereafter erected or structurally altered or repaired or remodeled in excess of fifty (50) per cent of the assessed valuation of such building, shall have a set-back of not less than ten (10) feet from the street property line on which the building fronts.

#### Capitol Boulevard Architectural Restrictions

Section 17. Every building hereafter erected or structurally altered or repaired or remodeled on lots facing Capitol boulevard for the entire length of Capitol boulevard and in Zone D shall be subject to such architectural restrictions as may be required by the Board of Adjustment. Before a permit is issued for the erection or remodeling of any structure within this portion or part of "D" Commercial Zone, the applicant shall file with the Building Inspector such plans and specifications as may be required to show the design of such proposed building or alterations.

#### Street Signs (Capitol Boulevard)

Section 18. Electric street signs shall not project more than four (4) feet and six (6) inches from the building lines along Capitol boulevard.

#### Roof Signs (Capitol Boulevard)

Section 19. Roof signs shall not project beyond the property line of set-back line along Capitol boulevard. They shall be illuminated and the design approved by the Board of Adjustment. Not more than fifty (50) per cent of such sign shall be solid area. Letter boxes to be fastened to skeleton frame-work and each letter to be illuminated as required of Class A, B, and C electric signs.

Passed by the Council and approved this 8th day of March, 1932.

ATTEST:  
ANGELA HOPPER,  
City Clerk.

J. P. POPE,  
Mayor.

Figure 20. 1932 City Ordinance restricting Capitol Boulevard's architecture and signs.

## IDAHO FORESTRY BUILDING

Located on the Boise River, the Idaho Forestry Building is one of the boulevard's most indigenous buildings. Designed by architect Hans Hulbe the rustic log structure was constructed by CCC crews and completed in May of 1940, helping to celebrate fifty years of statehood for Idaho.

Constructed of Idaho wood products the log and shingled gable roof structure is imposing in its park setting. The exterior logs used on the cabin's shell are Idaho Englemann spruce. This work was completed by two Finlander round log artists. These experts were loaned to the state by the Idaho Timber Protective Association where they were employed to build houses and towers for look-outs. The interior of each room has its own special wood pattern with the walls, ceilings, and floors covered with an individual native Idaho timber species, including the western white pine, ponderosa pine, western red cedar and Douglas fir. The materials were donated by various Idaho lumber companies.

Originally called the "Chateau de Bois" and now known as "the log cabin," the building first housed the office of the state forester. That office was consolidated into the Idaho Department of Lands, which now occupies the building's two floors. For a rental fee of one dollar the department holds a 99-year lease on the structure from the city, which took effect on March 13, 1939.

The triangular plot where the cabin sits was originally part of Julia Davis Park but was cut off from the main park by the construction of Capitol Boulevard in 1931. As originally proposed, the grounds were landscaped and planted with only shrubs native to the state of Idaho, such as sumac, syringa, aspen, wild honeysuckle, and wild rose. Over the years, a prize display of native Idaho timber species including Idaho's state tree, the western white pine, was added. Unfortunately, a number of the trees were lost with the expansion of the greenbelt path onto the property.

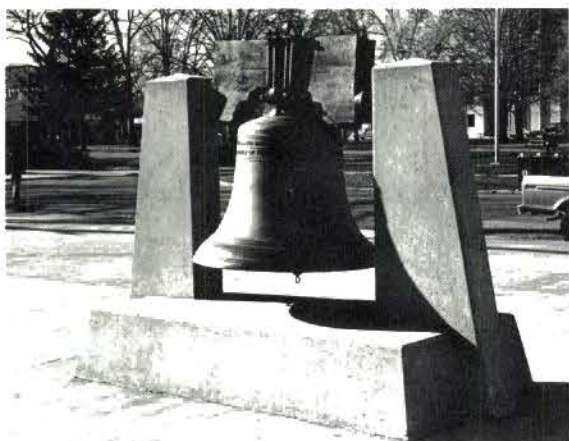


Figure 21. Idaho Forestry Building. Built in 1940 this log building was finished with Idaho wood species, honoring Idaho's 50th year of statehood.

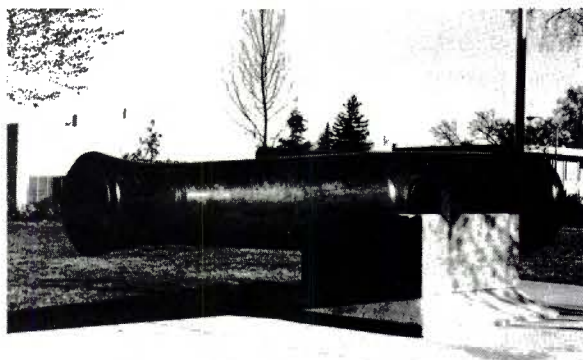


## STATUES, MONUMENTS, MEMORIALS AND WORKS OF ART

A variety of statues, monuments, memorials, and works of art grace Capitol Boulevard. Crafted from materials that include brass, stone, tile, cement, wood, or metal, they serve as a lasting reminder of a person, event, or historical moment in Boise's and the nation's history. The boulevard's first memorial was the George Washington Equestrian Statue, which was carved by Charles L. Ostner from native yellow pine and presented to Idaho Territory on January 8, 1869. Formerly located on the statehouse grounds, the statue was moved inside to the rotunda in 1935. Erected to perpetuate a continuing memory the monuments along Capitol Boulevard have an enduring value that adds to the boulevard's civic and historic experience. All statues, monuments, memorials, and works of art within the Capitol Boulevard Special District will be preserved, including periodic maintenance and cleaning.



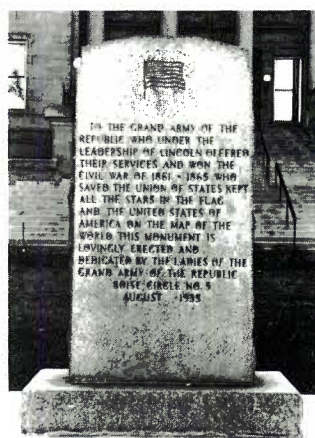
**1. LIBERTY BELL.** The bell is inscribed with "Proclaim Liberty throughout all the land unto all the inhabitants thereof. LCV XXVXX. By order of the Assembly of the Providence of Pennsylvania for the Statehouse in Philad. A. Pass and Stow Philad. A MDCCLIII."



**2. CIVIL WAR GUN.** Dated 1857, this 42-pound sea-coast gun was used during the Civil War at Vicksburg. It was donated to the State of Idaho in 1910.



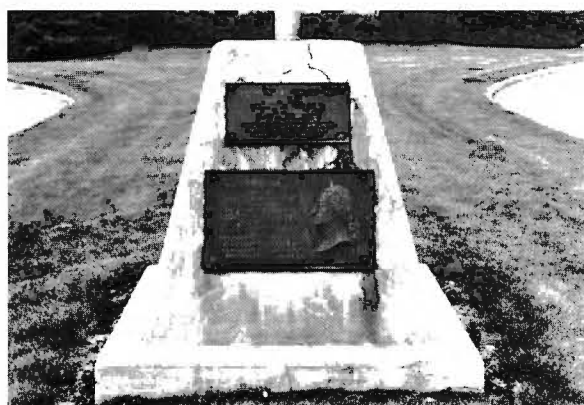
**3. PIONEER MONUMENT.** Erected in 1906 by 2,777 school children of Boise this monument serves to perpetuate the memory of the Old Oregon Trail and honor the pioneers who established the first American Government of Oregon.



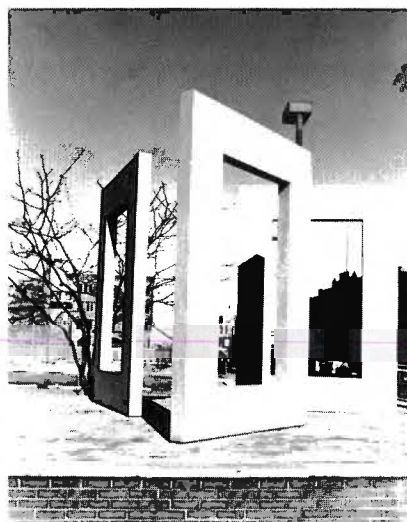
**4. GAR MONUMENT.** Erected by the Ladies of the Grand Army of the Republic in August, 1935. Dedicated to the "Grand Army of the Republic who under the Leadership of Lincoln offered their services and won the Civil War of 1861-1865 who saved the Union of States kept all the stars in the flag and the United States on the map of the world."



**5. GOVERNOR STEUNENBERG STATUE.** This memorial was erected in 1929 and is inscribed with "*Frank Steunenberg, Governor of Idaho. 1897-1900. When in 1899 organized lawlessness challenged the power of Idaho he upheld the dignity of the state, enforced its authority and restored law and order within its boundaries, for which he was assassinated in 1905. 'Rugged in body, resolute in mind, massive in the strength of his convictions, he was of the granite hewn.' In grateful memory of his courageous devotion to public duty the people of Idaho have erected this monument.*"



**6. WASHINGTON MONUMENT AND FLAGPOLE.** Erected by Salt Lake Lodge No. 85, B.P.O.E. and Boise Lodge No. 310, B.P.O.E. The monument was dedicated on February 22, 1936 to "*the everlasting memory of the Father of our Country, George Washington. It also reads 'Americans Be On Guard,' F.D. Roosevelt, President, U.S.A.' and 'Elks Onward!'*"", Jas T. Hallinan, Grand E.P."



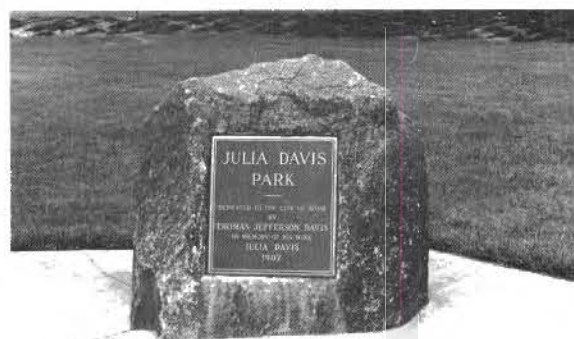
**7. POINT OF ORIGIN.** This trio of rectangular steel frames was designed by John Mason and located in front of Boise City Hall in 1983.



**8. TEMPERANCE FOUNTAIN.** Originally located in front of the former 1893 city hall, this memorial was erected in 1910 by the W.C.T. U. in memory of Mary B. Tolles. It was refurbished by the National Organization of Women and placed in front of the Boise City Hall in 1980.



**9. MCCLELLAN FERRY MONUMENT.** Erected by Pioneer Chapter, Daughters of the American Revolution in 1932, this monument, shaped like the State of Idaho, reads "*Near this spot the McClellan ferry crossed the river in 1864.*" Similar monuments were used to mark historic sites around Idaho in the 1930's.



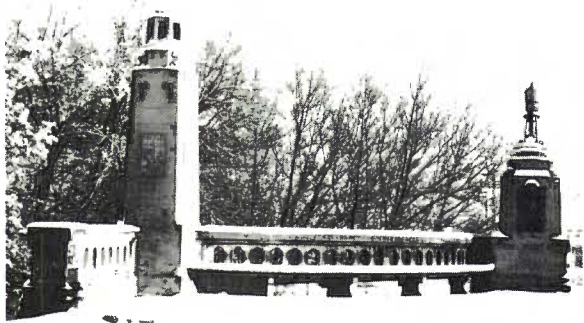
**10. JULIA DAVIS MONUMENT.** This rock memorial's brass plaque is inscribed with the words, "*Julia Davis Park, Dedicated to the City of Boise by Thomas Jefferson Davis, In Memory of his Wife Julia Davis, 1907.*"



**11. HUNT EXPEDITION MONUMENT.** This Woman's Christian Temperance Union's lava rock monument was erected and dedicated by the Sons and Daughters of Idaho Pioneers in 1931 in memory of the Wilson Price Hunt expedition, the first whites to visit Boise Valley (1811).

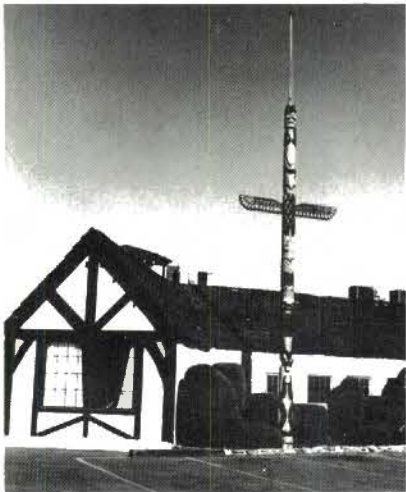


**12. TEN COMMANDMENTS.** This stone monument was presented to Boise City by Boise Idaho Aerie No. 115, Fraternal Order of Eagles, 1965.



**13. CAPITOL BOULEVARD MEMORIAL BRIDGE.** The bridge was "dedicated to the Pioneers of the Old Oregon Trail in 1930-1931." It was built by the State of Idaho Department of Public Works in cooperation with U.S. Bureau of Public Roads and the City of Boise. Contractor was Morrison Knudsen Co.

**14. JOHN BOOTH MEMORIAL PARK PLAQUE.** 1962 plaque missing. No photo available.



**15. TOTEM POLE.** Carved by Gordon Camby and topped with a flagpole, this is representative of those carved by Pacific coast Indian tribes. It was privately installed in the 1940's.



**16. DEPOT BELLS.** A brass plaque in the depot reads "Bells in this tower were given in memory of Edward Henry Harriman. Of all the great builders.. The famous doers of things in this busy world... None more ably and manfully did his appointed work."

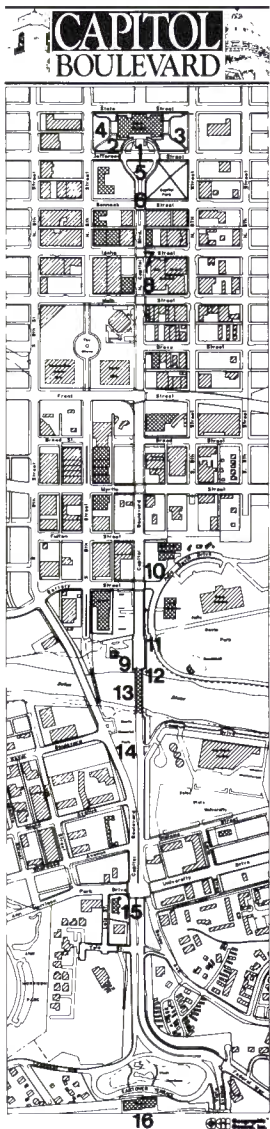


Figure 22.

## CAPITOL BOULEVARD ANALYSIS

### THE GRAND BOULEVARD

Capitol Boulevard is Boise's most impressive street. Originally conceived as a wide park-like approach to the state Capitol, the boulevard remains a memorable experience of Boise's special characteristics. With the Capitol and its handsome dome silhouetted by the Boise foothills the approach to the city is unequaled for beauty and grandeur. Crossing the Boise River on the well crafted Capitol Boulevard Bridge the boulevard passes adjacent to the grassy expanse of Julia Davis Park and its numerous cultural facilities, then cuts through the heart of downtown Boise and the formal Capitol Park, leading directly toward Idaho's Capitol. To the south lies the landscaped Platt Gardens and the imposing depot tower landmark that pierces the skyline. Motorists entering Boise will be struck by the panoramic view of the city before them. The visitor will be inclined to long remember the capital city because of that first glimpse.

Such perceptions are extremely important in reflecting the quality of city life in Boise. Reading the boulevard, one can judge the city's prosperity, sense its grandeur, review its history, count its opportunities, and determine its character, which is why it is essential to maintain the boulevard's special characteristics.

Designed under the influence of the Beaux Arts School of monumental neo-classical architecture the boulevard's plan is an expression of grandeur, spaciousness, and visual cohesiveness. The

boulevard's axial alignment terminates at each end with landmark architecture. The boulevard's approach, along with its adjacent parks and small landscaped spaces, offers a sense of spaciousness and a special interrelationship between its various uses and functions. Today, the opportunity remains to return Capitol Boulevard to a grand approach.

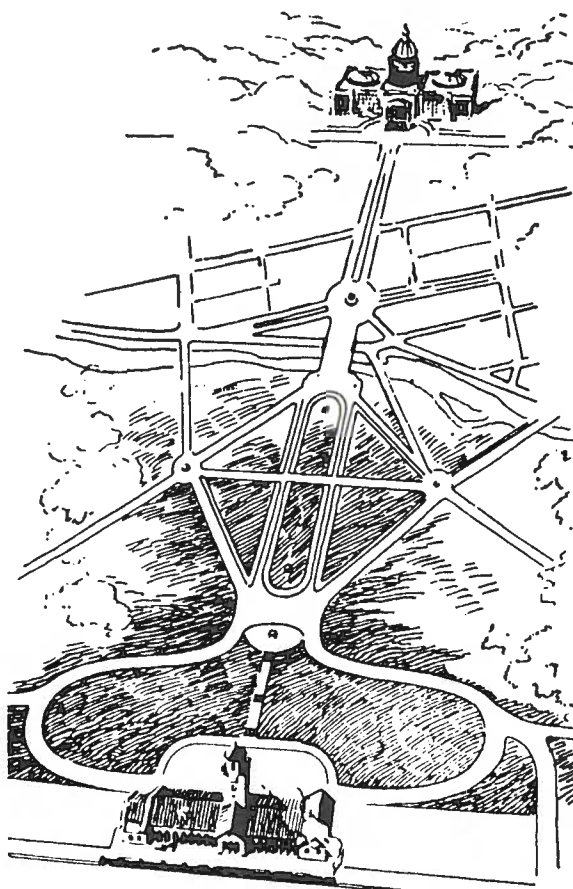


Figure 24. 1924 Grand Boulevard Plan.

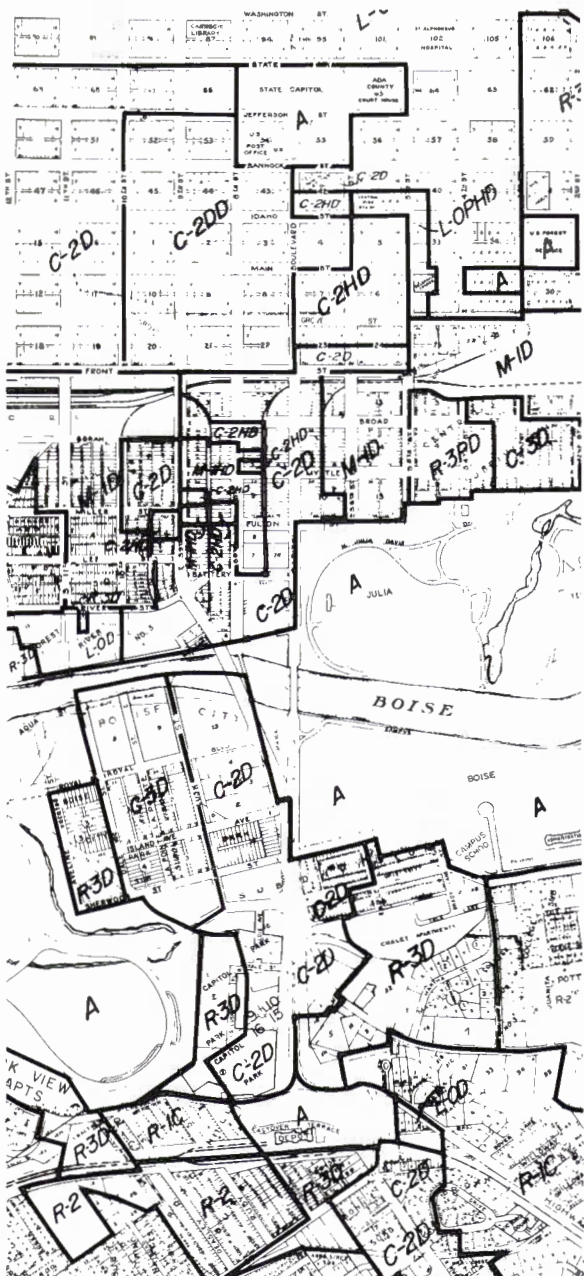


Figure 23. 1988 aerial view of Capitol Boulevard looking from the depot to the Capitol. Photo: Chet Bowers.

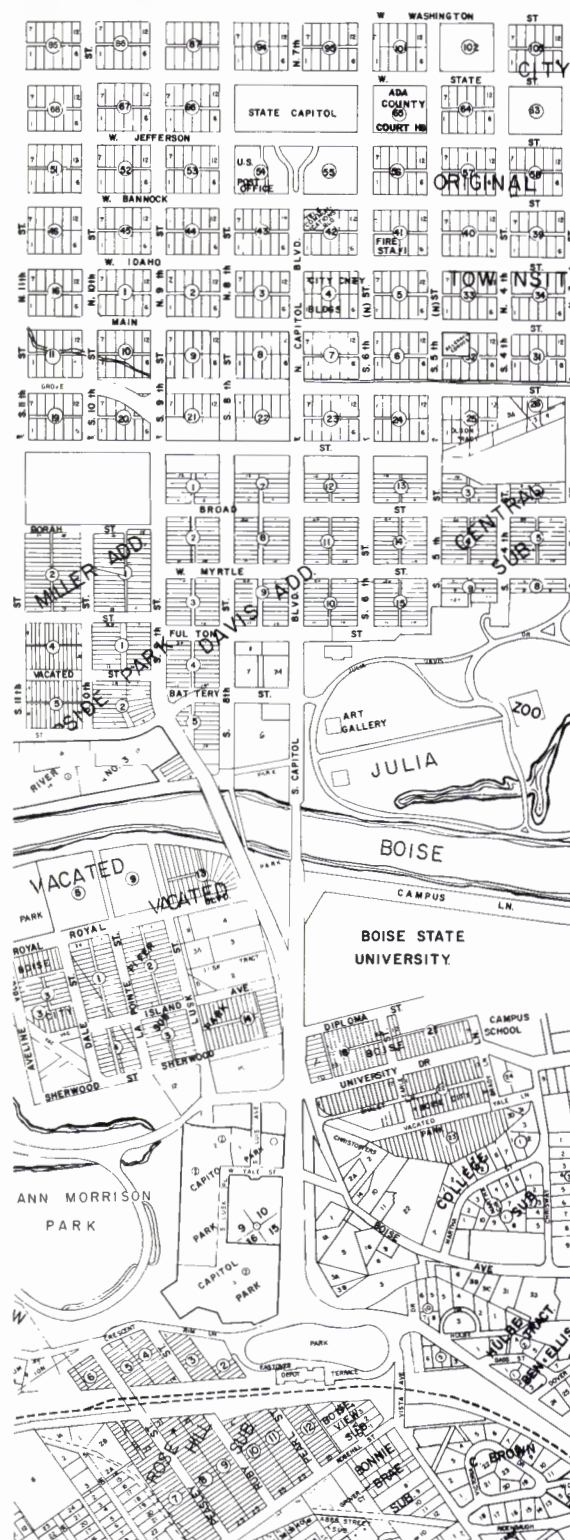
## LAND USE

Figure 25 illustrates the existing zoning in and around the study area. The great majority of property along the boulevard is zoned commercial or *C-2D*. This General Commercial Zone fulfills the need for travel related service and retail sales. The downtown core and Boise Redevelopment Area are zoned *C2-DD*, a Downtown Design Review District. The two historic areas are zoned *HD* for historic design review. The entire boulevard falls under design review regulations, which have been established to insure that the general appearance of the development of the land shall not be in conflict with the Comprehensive General Plan or other development plans adopted by Boise City for specific areas.

Large areas along the boulevard are zoned *A* or open land, including the Capitol area, Julia Davis and Ann Morrison parks, Boise State University, and the depot grounds. Zoned uses at the southern end of the boulevard are primarily residential, ranging from single family to multi-family. Residential activity also occurs with the concentration of motels near the University Drive intersection.



**Figure 25.** Zoning Map. The primary zoning classifications on the boulevard are *C-2D* (commercial) and *A* (open land).



**Figure 26.** Boulevard additions and platting. Notice grid block and street pattern north of the river and varying patterns to the south.

The boulevard is also affected by nearby *C-3D* and *M-1D* zones located near Ann Morrison Park and the old warehouse area that often negatively impact the boulevard with heavy truck use. Their haphazard development does not fulfill the potential of the boulevard and surrounding downtown development.

The map illustrated in Figure 26 represents the various additions and platted properties forming the boulevard's edge. The parcel sizes differ from very uniform sizes north of the river to larger lots and a more north-south platting south of the river.

## CIRCULATION

With the boulevard's major vehicular circulation improvements recently completed, the timing is now right to refine the boulevard's edges that have been in transition for many years. Capitol Boulevard's and the downtown's vehicular circulation, which has been under study for over twenty years, is now nearing completion and will be providing service to the city center for years to come. (See Transportation, page 8.) These include the expansion of the depot hill approach connecting to Vista Avenue and Federal Way in 1985 and the opening of the 9th Street Bridge in 1988. With completion of the new bridge, north-south traffic was converted to a one-way couplet north of University Drive with 9th Street carrying southbound traffic and Capitol Boulevard transporting northbound traffic.

The east-west transportation corridor, due for completion in 1992, will move westbound traffic on Myrtle Street and eastbound traffic on Front Street.

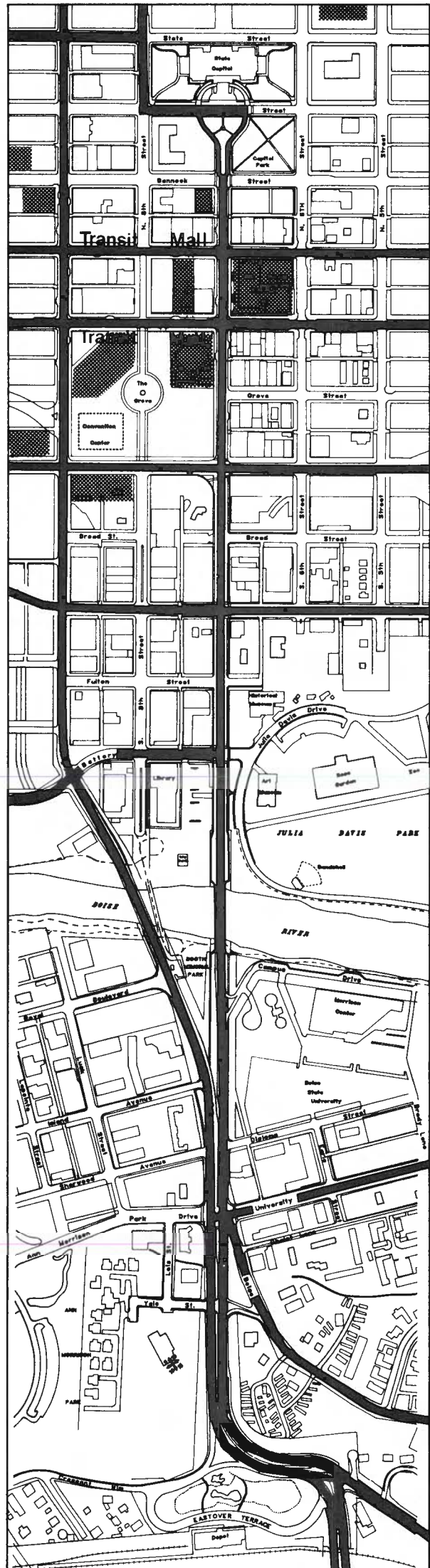
Designed to improve flow and safety, Capitol Boulevard's arterial traffic operates well. Providing traffic movement to and within the city center the boulevard feeds into the city's arterials and regional highway network, connecting directly to Interstate 84 via Federal Way and Vista Avenue. The city's other arterial entrances penetrate the city and converge near the boulevard.

The expanse of the boulevard roadway varies from four to eight lanes, carrying heavy traffic volumes with an average of over 16,180 vehicles crossing the river each day. The volume of southbound traffic is slightly greater than northbound. The combined north-south average daily traffic count at the University Drive intersection is 36,000 vehicles.

North of the river, Capitol Boulevard has a right of way width of 80 feet. South of the river, the widths of rights of way vary due to the angle alignments of South 9th Street, Boise Avenue, and the grade of depot hill.

The entire length has curb and gutter with actual roadway widths varying from two 20-foot roadways around the Steunenberg monument to 52 feet from Bannock Street to Front Street, 60 feet from Front Street to the river, and widening to 90 feet at the University Drive intersection with its two left turn bays. Roadway width is 97 feet on Depot hill, including a seven-foot median with mountable curbs that have decorative paving and landscaping.

Projections for year 2000 traffic volumes by the Ada Planning Association identified that average daily traffic at the river will decrease slightly due to an improved arterial network. However, boulevard traffic passing directly into the downtown blocks will increase.



**Figure 27. Arterial circulation.**

One present traffic problem on Capitol Boulevard occurs at the intersection of University Drive. Peak southbound traffic experiences delays due to the long signalization required at this five-sided intersection and the influx of traffic caused by the activity of Boise State University. Though better synchronization may improve this intersection, further study of ways to improve traffic flow is required.

Proposed landscaping for Capitol Boulevard would enhance the approach of the Capitol and reinforce the traffic functions. Treatment of signs, signals, fixtures, street lights, and crosswalks would be consistent with good traffic engineering practices and with the aesthetic objectives of the boulevard.

### Parking

In most cases, on-street parking is prohibited on Capitol Boulevard to facilitate traffic flow. A few on-street spaces are located just north of the bridge and in front of Boise City Hall. Other pull-outs located in the downtown area are intended for loading and unloading.

Off-street parking to serve the boulevard's activities is located in a number of parking garages and numerous at-grade parking lots. Garages on the boulevard include the four-story Key Financial Center garage (143 spaces), the City-County building (232 spaces) and the Idaho First Plaza garage (240 spaces) which enters and exits onto the boulevard. The new 522-space Capitol Terrace garage, which exits onto Capitol Boulevard, has been integrated into the block's development. Most of the boulevard, with the exception of the downtown area, is served by at-grade parking lots. A number of these lots along the boulevard lack landscaping, thereby diminishing the boulevard's character. Parking, though always in demand, is generally sufficient to assure the viability of existing development.

### Bus

The boulevard's transportation function is strengthened by a new transit mall. The transit mall, located along Main and Idaho streets between Capitol Boulevard and 9th Street, creates a terminus for improved city bus transportation and adds to the downtown's people-oriented activity. The transit mall's handsome streetscape and furnishings encourage use. The boulevard's circulation plan is compatible with current and future bus service requirements, providing flexibility to changes in routes and schedules. Northbound bus stops are located across from the Boisean Motel, B.S.U., and Battery, Myrtle and Front streets. Southbound stops occur at Royal, University, and Yale. A number of bus shelters and benches are located along the boulevard. Future consideration needs to be given to improving facilities (i.e. shelters, bus bay) to better serve Boise State University..



**Figure 28.**  
Amtrak Guide Sign

### Amtrak

Capitol Boulevard plays an important role in linking Boise's transportation hubs. Via Vista Avenue, Capitol connects the Boise Air Terminal and Amtrak with the downtown. Amtrak provides an early morning westbound and late evening eastbound train, serving Boise. This limited service resulted in approximately 20,000 passengers getting off at Boise in 1987, an average of 50 passengers a day. The ticket office (open on a limited schedule) is maintained in the waiting rooms of this handsome Spanish style depot.

### Pedestrians and Bicyclists

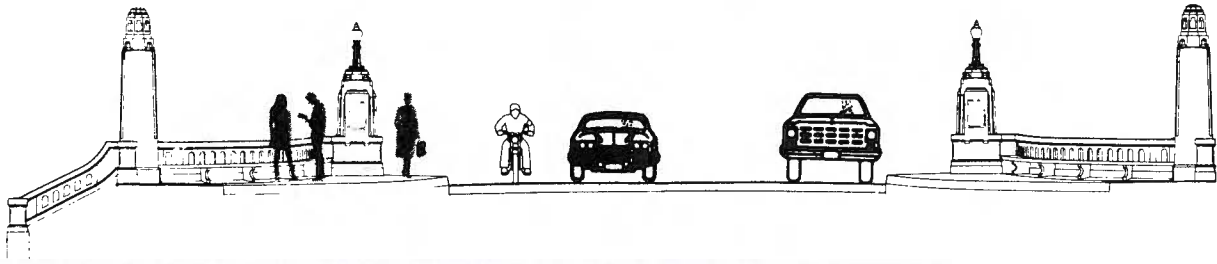
Many of the responses to the boulevard questionnaire were that the Capitol Boulevard's pedestrian and bicycle systems are poorly defined and often unsafe. Though the boulevard has been primarily designed for driving, it needs to strike a more equitable balance to serve pedestrians and bicyclists. The demand for improvements to the boulevard's sidewalk system will continue to grow from the increase of activities at Boise State University, the area's motels and restaurants, the greenbelt, the numerous parks and cultural activities, and the downtown center of commerce and employment.

The Boise River greenbelt provides the boulevard with an excellent east-west connection for pedestrians and bicyclists. The greenbelt crosses the boulevard under the Capitol Bridge on the south side of the river, while the north side utilizes the Battery Street signalized intersection, connecting to Julia Davis Park. The 8th Street Bridge provides the connecting path to comfortably cross the river.

Sidewalks on the boulevard vary in width from five to 14 feet. The new sidewalks from Bannock to Front are 14 feet wide. From Front Street to the Boise River most sidewalk area is ten feet. South of the river, sidewalks are generally six feet wide with a portion of the east side of the depot hill being ten feet and separated from the boulevard by an eight-foot landscaped strip.



**Figure 29.** Narrow six-foot sidewalk adjacent to traffic lane by Julia Davis Park.



**Figure 29.** Capitol Boulevard Memorial Bridge crossing the Boise River.

In most cases, the sidewalk is adjacent to the curb with heavy volumes of traffic making pedestrians and bicyclists feel vulnerable. Also, the boulevard's width of four or more lanes can prove difficult to cross. These conditions, as well as 35 mph speeds, often discourage pedestrian and bicycle use and contribute to the current low volume of pedestrian activity along the boulevard. Crosswalks are located at Jefferson, Bannock, Idaho, Main, Front, Myrtle, Battery, University and Federal Way. The pedestrian crossing at the Battery Street intersection has been identified with special paving.

Improvements to the boulevard's pedestrian system is a critical element in making the boulevard a more lively place and linking all of its interconnecting activities. With the boulevard's vehicular circulation system now in place the refinements to pedestrian and bicycle systems can now be implemented. South of the Boise River, Capitol Boulevard serves as part of the city's bicycle system providing a linkage to and from the bench. Curb cuts have been added to encourage bicycle use on the parallel sidewalks. Commuter cyclists often utilize the street.

North of the Boise River, 8th Street has established itself through landscaping, lighting, and pedestrian amenities as the primary pedestrian/bicycle spine through the downtown. South of the river, Capitol Boulevard is the primary north-south pedestrian and bicycle route.

### NATURAL FEATURES

The boulevard's topography slopes gently toward the river with Capitol's elevation of 2704 being 26 feet above the river's elevation of 2678. Southward, the boulevard again rises gently, then climbs the first bench plateau at a height of 79 feet to reach the depot's elevation of 2757.

The boulevard crosses two natural drainages. These are the Boise River and the Budd Canal slough, which is located just south of the Totem Building and drains the area parallel to Boise Avenue into Ann Morrison Park. The Grove Street Ditch passes under Capitol Boulevard at Grove Street, then flows under the west side of the boulevard before it leads westward on Front Street.

The Boise River provides the boulevard with a natural amenity that contributes significantly to the variety of experiences found on the boulevard. Besides offering relief from the formal character of the downtown, the river front allows visitors to experience one of the city's finest features. Clear and

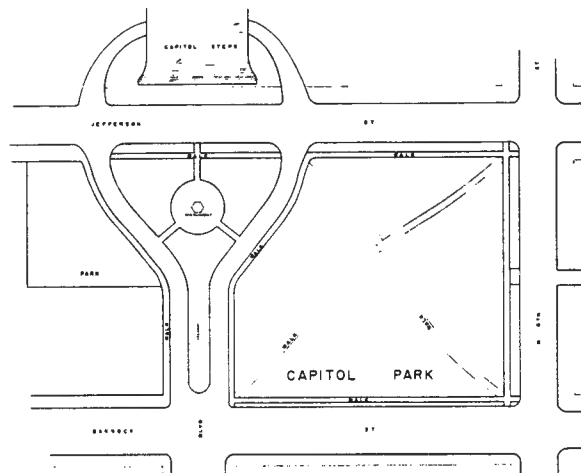
unpolluted with its origins in the Sawtooth Mountains, the river is a haven for fish and wildlife, particularly waterfowl. The open flowing water, which has its flow controlled by Lucky Peak Dam, normally runs between 2,000 and 7,000 cubic feet per second, depending on run-off and irrigation needs. The river banks near the boulevard have been straightened, channeling the water; however, a couple of small islands and sand bars can be found in the area.

### PARKS

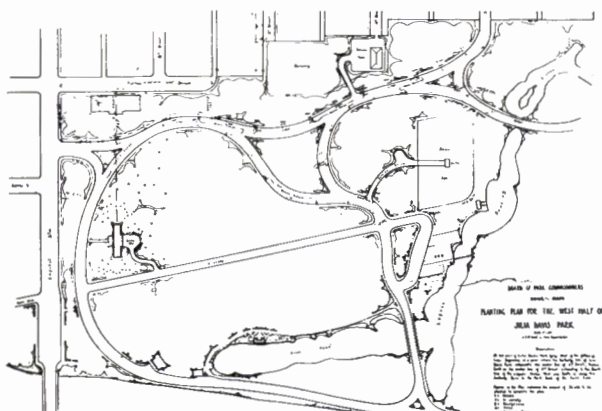
Capitol Boulevard is fortunate to have a strong presence of parks at both ends of the boulevard, as well as centered along the Boise River. Six parks (Capitol, Julia Davis, Boise River Greenbelt, Booth Memorial, Ann Morrison, and Platt Gardens), as well as a number of small park islands, contribute greatly to the characteristics of the boulevard and its cultural and recreational functions. A listing of the boulevard's parks and their history follows:

#### Capitol Park

Opened by the city in the 1920's, Capitol Park is part of the spacious approach to the Capitol, adding to its grandeur and dignity. Featuring a curved roadway approach and the stately Steunenberg Monument, this two-block park—a grassy expanse with mature trees—is a popular lunch spot. Its history is reviewed on page 6.



**Figure 31.** The Capitol Park design ties in with the Capitol grounds.



**Figure 32.** Julia Davis Park plan for the motor age by park superintendent J.T. McLeod in 1931.

### Julia Davis Park

Julia Davis Park was conceived of by Tom Davis, who wished to establish a memorial to his wife and at the same time give Boise a recreational spot within close proximity to its residents. Mr. Davis donated land for the park in 1907. He died a year later never having seen the park but was assured of its inevitability by restricting that the land must be used only for a public park. (See Figure 17.)

Boise's Columbian Club factored in getting the park started with plans for a park on both sides of the river, including a park island; however, they were never carried out. Actual work on the park began in 1910 when A.V. Eichelberger was appointed park commissioner and sponsored the first grass seeding and tree planting in the western end of the park. Also at that time, the road bordering the park on the north was surveyed and elm trees were planted in column form along it. This straight east-west drive through the park is one of its early design features and led to the original entrance to the park on South 8th Street. When Capitol Boulevard was opened in 1931 it cut through the western end of the park, separating a small block along the river where the Idaho Forestry Building now sits and opening a new park entrance off of Capitol. Originally, the entrance had been on South 8th Street where it was graced in 1926 by a formal entrance with cement piers and a wrought iron gateway. These cement columns were recently uncovered near the original site. Today, this replanted tree lined walk is the center of the annual Art in the Park festival.

Over the years, the park has expanded from 40 to 86 acres, extending to Broadway Avenue. Much of the park ground was reclaimed with yards of dirt required to level out the park and protect it from destruction by high river currents. In the 1920's, a drive along the river was built and planted with shade trees and shrubbery. During the development of the park the central area was laced with canals and lagoons, providing facilities for boating and fishing.

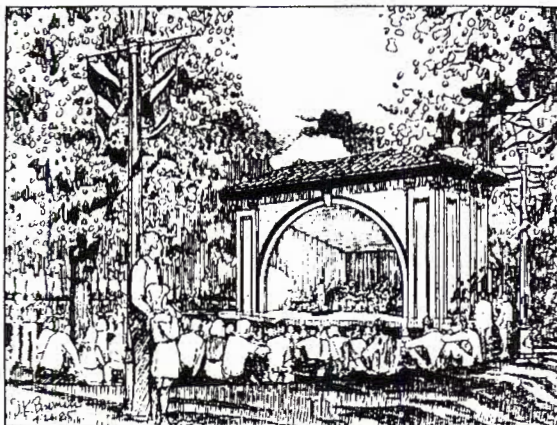
Large areas of the park were seeded to grass and numerous trees were planted. The Columbian Club was responsible for planting many of the park's early trees. The lush park now has over 1000 mature trees and is widely known for its broad lawns and shade.

Julia Davis Park has always been known as one of the city's most beautiful spots and provides a wealth of benefits to the entire community.

The park's western end features the city's growing cultural center with the Boise Art Museum, Idaho Historical Museum, and Band Shell, as well as the city library across the boulevard. The park's many special uses include the following:

**The Zoo--**The zoo was started in 1916 and was located near the center of the park where the bandshell now stands. The zoo was supplied with its first animal when a monkey, having escaped from a circus passing through Idaho, was caught on the desert between Boise and Mountain Home. After a few years, the zoo was moved to its present location near the center of the park and expanded. Overseen by the Idaho Zoological Society and aided with plans prepared by Bob Hamill the zoo underwent substantial improvements in the 1960's, 1970's, and 1980's, becoming a major attraction to families of the region.

**Bandshell--**The bandshell in Julia Davis Park was built in 1928 from a design by Wayland and Fennel. Constructed in Spanish Mission style similar to the Union Pacific Depot it soon became the locale for weekly concerts. The bandshell was partially refurbished and landscaped in 1986.



**Figure 33.** 1928 Spanish style Bandshell.

**Rose Garden--**A municipal Rose Garden was established in the park in 1932. It now holds over a thousand varieties of roses and is recognized by the American Rose Society as a testing garden for new varieties.

Other features of the park include the Fun Spot and the new Discovery Center, a children's science museum located in the former Navy reserve building on Myrtle Street.

## The Boise River Greenbelt

Following the Boise River, the greenbelt and its bicycle and pedestrian paths straddle the flowing water, providing an east-west river linkage to the surrounding city and county. Segments near Capitol Boulevard, initially begun in the early 1970's, expanded with the North Bank Project in 1978 and again in 1988 when the 8th Street Bridge was converted to non-motorized use and new approaches were built connecting the bridge to the greenbelt. The path crosses under the Capitol Bridge and South 9th Street on the south side of the river and connects north and south routes via the 8th Street Bridge. The greenbelt system continues to expand outward and the growing use and appreciation by the community make it one of the city's finest resources.

### Booth Memorial Park/Tourist Rest Stop

This triangular park, which is located between Capitol Boulevard and South 9th Street on the south side of the Boise River, is now really a part of the greenbelt. Called Booth Memorial Park or Tourist Rest Stop, the park has been slightly expanded with the realignment of South 9th Street and the inclusion of the historic 8th Street Bridge. (See page 8.) The park is extremely visible from both Capitol and South 9th Street. It is at the southern point of the triangular park where Capitol Boulevard becomes a one-way northbound street. Additional landscaping, including a row of red oaks and scattering of crab apple trees set within grass turf, makes the park a visually pleasing sight to passing motorists.

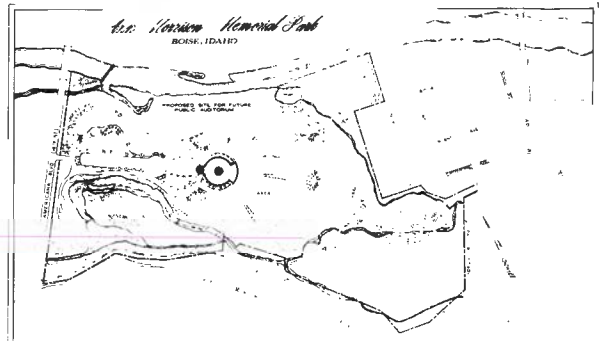
Access to the park is limited and primarily designed for pedestrians or bicyclists. The park is accessible from the greenbelt, 9th and Capitol bridges, or the 8th Street Bridge, which was renovated for use by pedestrians and bicyclists in 1988. With new approaches the bridge provides good circulation connections for the greenbelt and directly links 8th Street to the downtown. The greenbelt mileage marker system starts at 0 at this park.

The park has a small parking lot just south of the 9th Street Bridge. A former parking area near this one gives the park one of its names as Tourist Rest Stop. Often tourists would stop at this point to check their map and bearings and stretch their legs by walking down a set of stairs to the river.

In 1968, the park was named after John Booth, the former director of the Idaho Education Association and active member of the Idaho Association of Garden Clubs. At the time, garden clubs planted shrubs along the river; however, with the expansion of the greenbelt system in the area many of these shrubs have been lost. With all of its recent modifications the park is still in a period of transition.

### Ann Morrison Memorial Park

Dedication of Ann Morrison Memorial Park was on June 7, 1959. Roughly 3/4 of a mile long and half a



**Figure 34.** 1959 plan for Ann Morrison Park was oriented towards Americana Boulevard rather than Capitol Boulevard.

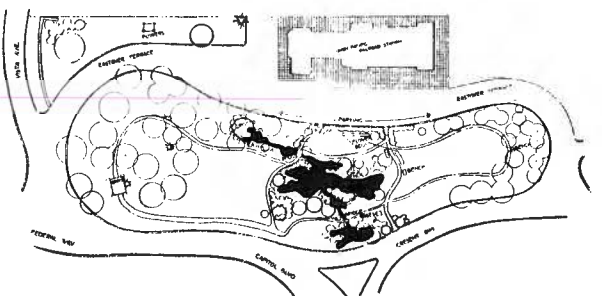
mile wide, the park parallels the Boise River. As illustrated in Figure 34, the park was designed with an orientation off Americana Boulevard rather than Capitol Boulevard. Boulevard access is from Ann Morrison Park Drive or Royal Boulevard.

The 153-acre site was purchased from the Boise School Board by the Harry W. Morrison Family Foundation. Some 30 acres are devoted to sports, including a baseball diamond, tennis courts, horse-shoe courts, and an archery field. Forty acres paralleling the cool dells along the river are set aside for picnics and family entertainment, featuring barbecue facilities and playground equipment. Another 30 acres, originally intended for boating and water sports, have become home for water fowl. This area also features an island and wooden bridges. Another feature of the park includes a 1000-foot long mall that is lined with trees and leads to a reflecting pool graced by an illuminated fountain and surrounded by a garden. The large circular setting in the park was reserved for a civic auditorium, which was eventually built on the B.S.U. campus. Landscaping of the park was undertaken by Charles Davidson with many of the trees donated by local residents and the park department. Over the years, the street design has been modified and the trees have matured, creating a most pastoral setting.

The park was a tribute to the wife of Harry Morrison, who was co-founder of Morrison-Knudsen Company. Ann Morrison was nationally known as "the first lady in construction." Her memorial is located near the fountain.

### Platt Gardens

Landscaped in 1927 by the Union Pacific Railroad, Platt Gardens is one of the boulevard's finest parks. Set on the bench plateau, the park provides an



**Figure 35.** Platt Gardens Site.

impressive view of the city and an oasis of greenery surrounding the depot. The park features a monument of rock with a stream of water flowing along a rock grotto into a series of ponds. Connected by a winding walk, the park is appointed with handsome lights and served by a large parking area adjacent to the depot. (See page 7.)

Small Park Spaces

There are a number of small parks located south of the Boise River along Capitol Boulevard. Though relatively small, these public park parcels play a major role in the enhancement of the boulevard's edge. Such spaces on the west side include public portions of the former South 8th Street pike alignment that angles into Capitol near Sherwood Street. Portions of this alignment (i.e. directly in front of the Boulevard Motel) feature a sidewalk, turf, and a few trees. Park property on the east side contains a strip along the frontage of Boise State University and a small Boise Avenue triangle located in front of the Boisean Motel. Planted with turf, plums, and sweet gums, this park space is part of the original alignment of Boise Avenue as it angles into the boulevard. Other public park property is found along the edge of depot hill. This includes landscaped park property across from the Platt Gardens where Capitol Boulevard intersects with Federal Way and Vista and property running to the foot of the hill. A few landscaped traffic islands, as well as the median on depot hill, serve to channelize traffic.

TREES AND VEGETATION

As a part of the study, the existing landscape conditions within the study area were surveyed. The Capitol Boulevard Woody Plant Survey, conducted in the spring of 1988, identified species, locations, sizes, and conditions of trees and shrubs presently found within a one-block radius of the boulevard. A total of 454 trees have been identified, indexed, and mapped. This figure does not include trees located in The Grove, Boise River riparian plantings, greater Julia Davis Park, or recent plantings in Booth Memorial Park. The boulevard's trees comprise more than fifty species with honey locust, Norway maple, hawthorn, sweet gum, red oak, and plum occurring most often. More than 25 species of shrubs were also identified. With the exception of most park areas, the majority of trees are quite young.

A number of historical or rare trees can be found along the boulevard. Most noteworthy is the presidential grove on the statehouse grounds. The first tree to be placed in the grove was a magnificent water oak, planted by President Benjamin Harrison on May 8, 1891. Unfortunately, it has been badly trimmed. Next to it stands a Norway maple planted by Theodore Roosevelt on May 28, 1903. The third is an Ohio buckeye (horse chestnut) planted by President Taft on October 9, 1911.

A rare Ginkgo biloba, which was planted in the early 1920's, sits on the left side of the main entrance to

the statehouse. Numerous other trees on the grounds have been planted to honor an individual or to serve as a memorial. Most are identified by a plaque. Statehouse trees have been planted in honor of organizations, such as the Grand Army of the Republic and Sons and Daughters of Idaho Pioneers; individuals, such as Governor C. Ben Ross and Martin Luther King; and special events, such as Golden Jubilee, the Bicentennial, and a moon tree from Apollo Eleven's voyage to the moon.

Other rare trees honored by the Urban Forestry Unit of the Boise Park System are a cucumber magnolia located in Capitol Park and a Kentucky coffee tree located on the western end of Julia Davis Park. Planted by W.E. Pierce in 1907, this tree is recognized as being the largest of its kind in the state. Pierce, a former mayor and early developer, was responsible for planting thousands of trees along Boise's streets. Another tree found on both the Statehouse grounds and the former State Forestry Building grounds is Idaho's state tree, the Western White Pine.

Running perpendicular to Capitol Boulevard is the Boise River and its riparian areas. This area contains many different species of both native and naturalized trees and shrubs that are not only aesthetically attractive but provide an important wildlife habitat.

Other major tree plantings near the boulevard include the phased replacement of American elms with

TABLE 1

BOULEVARD SPECIES  
(Currently identified within one block of Capitol Blvd.)

Trees	Shrubs
Mountainash	Amur maple
Beech	Arborvitae
Pear	Barberry
Crab apple	Boxwood
Plum	Coralberry
Linden—American, Littleleaf, Silver	Cedar
Birch, White or Paper	Cotoneaster
Ash, Green	Dwarf ninebark
Catalpa	Double syringea
Cherry	Euonymus
Russian Olive	Forsythia
Elm—American, Siberian, English	Juniper
Kentucky coffeetree	Lilac
Ginkgo	Mahonia
Hawthorn	Mugho
Horse chestnut	Plum
Honey locust	Potentilla
Goldenrain	Privet
Maple-- Amur, Japanese,	Rhododendron
Norway, Silver, Sycamore	Rose
Magnolia	Spirea
Oak-- Red, White,	Sumac
Swamp white, Scarlet, Pin	Viburnum
Larch	Wisteria
Juniper	Yew
Fir, White	Yucca
Douglas fir	
Spruce—White, Colorado,	
Norway, Picea	
Pine— Austrian, Ponderosa, White	
Sweetgum, American	
Plane tree, London	
Tree of Heaven	
Tuliptree	
Walnut, Black	



**Figure 36.** The boulevard entrance to the Capitol is set off by a three-story portico with a Roman inspired pediment supported by four large Corinthian columns.

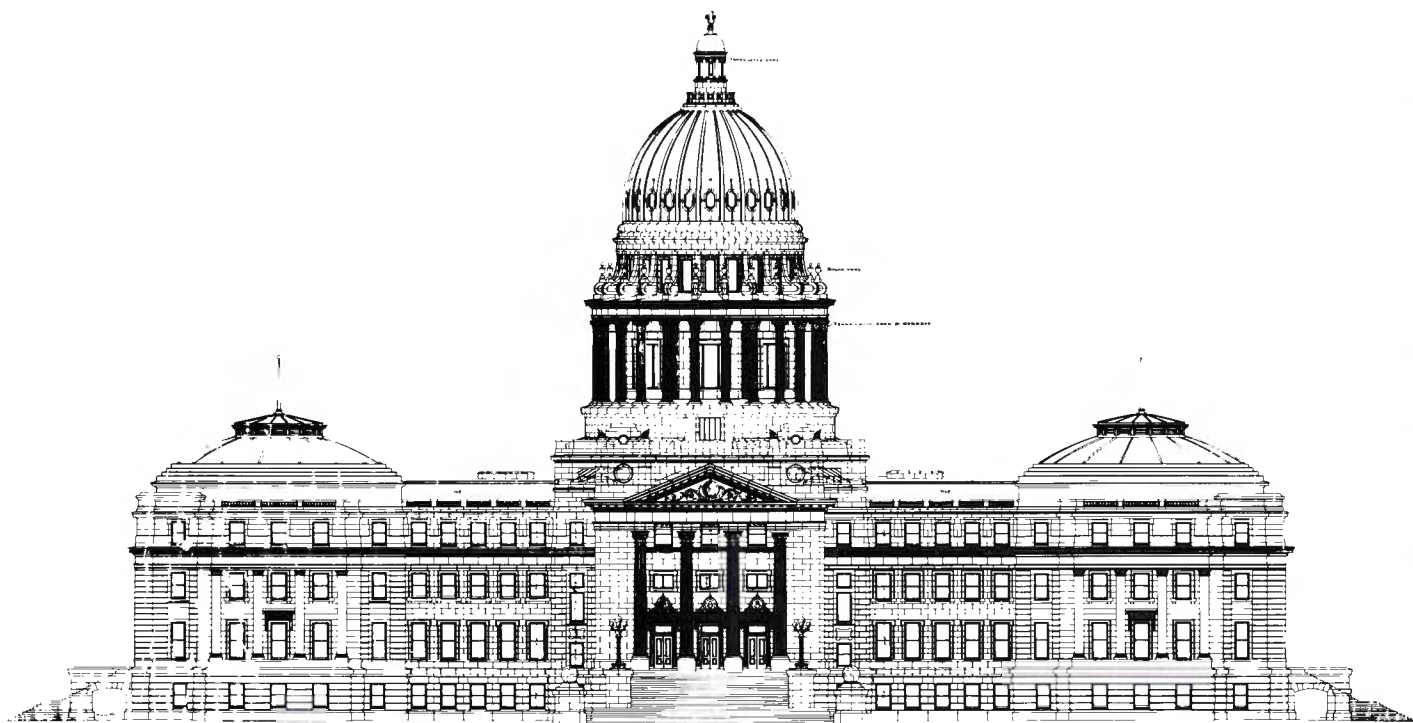
London plane trees on Park Drive in Julia Davis Park. The Grove features the symmetrical planting of 56 honey locusts in 1986. Other street tree planting projects include placing ginkgo trees the entire length of South 8th Street and along part of Battery Street in 1978 as part of the Northbank project, and planting the length of South 9th Street with ash trees in 1988 when rebuilding the road. Most recently, Booth Memorial Park was landscaped with a double row of red oaks along Capitol Boulevard and 9th Street and a central grove of pink profusion crab apples and Bechtel crab apples. Just being introduced along the boulevard in the Boise Redevelopment Area are columnar Norway maples on 21-foot centers. These trees and others planned for the boulevard will provide a great visible impact and help to define and humanize the boulevard space.

## BOULEVARD ARCHITECTURE

Capitol Boulevard comprises a large number of architecturally and historically significant buildings. The two dominant landmarks that shape Boise's skyline and terminate the boulevard at each end set a magnificent tone for the boulevard's architecture. Representing the crowning glory of the boulevard's architecture, the Idaho State Capitol is an edifice of splendor and proportions. The importance of this building cannot be overemphasized since the state-house remains the unquestioned symbolic focal point of government in Idaho. Patterned after the nation's Capitol the building represents the monumental neo-classical architectural style. The central section with its distinguished dome was built between 1906 and 1912. The flanking four-story wings were completed in 1921. (See page 5.)

The sculptural arched dome, which rises 200 feet, is capped with a bronze eagle and forms the major visual feature of the boulevard and the city. The dome is supported on a grand colonnade of Corinthian columns, forming a splendid interior rotunda space. The base of the dome is approximately 80 feet above the street. Much of the building's superstructure is faced with Boise sandstone quarried from nearby Tablerock.

Rising on the bench plateau just over a mile away the Union Pacific Depot stands opposite the Capitol on a radial axis. The striking white and red Spanish colonial style depot serves as a familiar Boise landmark set within the green Platt Gardens. Piercing the skyline is the depot's bell tower, featuring a Roman numeral clock facing the boulevard. The square walls of the campanile form an octagonal tower near the top with arched openings for the chimes. Built in 1925, with white stucco walls and orange tile roofs, the building was elaborately trimmed with local carved sandstone. (See page 6.)



**Figure 37.** The Idaho State Capitol is the focal point of Capitol Boulevard.

Numerous other significant buildings grace the boulevard and have continued to reinforce the fine architectural character deserving of the Capitol's stature. An inventory of historic buildings fronting Capitol Boulevard is listed in Table 2.

Important buildings include the old Federal Building across from the Capitol. Built in 1904, it was designed by the U.S. Treasury Department while James Knox was supervisory architect. The four-story stone and white brick building is a fine example of Beaux Arts architecture. The first floor has round arch windows and a similar doorway with excellent wrought iron decorations. In 1930 a substantial addition was built stylistically similar to the original. Across the street the six-story brick and terra-cotta Idaho Building is being rehabilitated as is the nearby handsome, heavily rusticated, sandstone faced Fidelity-Union Block. Another significant boulevard building is the 1927 Egyptian Theater. Rehabilitated in 1978, the movie palace is an outstanding example of Egyptian revival architecture.

The boulevard helps tie together three historic districts listed on the National Register of Historic Places, including the Boise Capitol Area District, Boise Historic District, and the South Eighth Street District. Running two blocks along Capitol Boulevard and surrounding the city-county building the Old Boise District contains the largest number of historically significant commercial buildings in the city. Most of the buildings were constructed during the period of the 1890's through 1920's and are typically two-story brick buildings with stone trim. Commercial buildings were built up to the street property line along the edge of sidewalk. Containing a mixture of architectural styles the district reflects an overall unity and cohesion due to similar building heights and the predominant use of brick. The Perrault Building located at Capitol and Main was built in 1879, making it the oldest commercial building in the city. The part of the district centering on Grove Street includes a number of structures of Basque heritage, including the Anduiza Pelota Court.

The South Eighth Street District is centered one-block off of Capitol Boulevard and consists of three

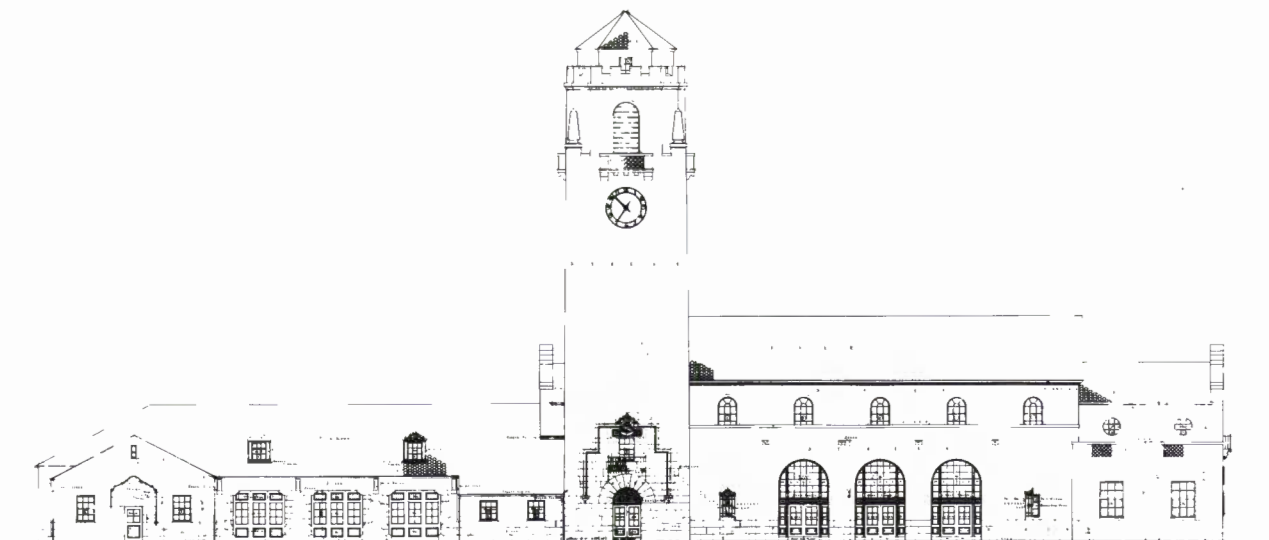


**Figure 38.** 1904 Beaux Arts style Federal Building. c. 1915.

blocks of turn-of-the-century warehouses. Rendered in brick, the warehouses have a level skyline and range from one to four stories. Much of the warehouse district was recycled into the 8th Street Marketplace specialty shopping area in the late 1970's and early 1980's. The district's size, character, and brick finish distinguish it from the surrounding area. The warehouse district is most visible from Capitol yet the historic district does not read well as its alley side dock area has been altered, concealing the rich brick character of the district, and is lost in a half-block of unlandscaped parking lots.

The proclamations of beauty and grandeur that helped open the Capitol Boulevard Memorial Bridge in 1931 led to passage of Ordinance No. 1589 on March 8, 1932 by the City Council and Mayor J.P. Pope. (See page 10.) This zoning law includes special architectural and sign restrictions: "Every building hereafter erected...or remodeled on lots facing and for the entire length of Capitol Boulevard...shall be subject to architectural restrictions..". The applicant was required to submit plans to show the design of such proposed buildings for the review of the Building Inspector and the Board of Adjustment.

The primary architectural restriction was that every building hereafter erected or remodeled would have a setback of not less than ten feet from the street property line. Restrictions were also placed on the location of gasoline pumps, signs, and billboards. Following this period, buildings, such as the art deco style Westinghouse Building located just south of



**Figure 39.** 1925 Spanish Colonial style Union Pacific Depot.

Front, were set back from the boulevard. Other one-story buildings, such as the Dunkley Music building and the former auto showroom on the Fulton Street corner, were similarly set back. Unfortunately, the regulations regarding signs and gas stations were softened over the years, allowing signs to project into the setback and gas pumps to be placed in front of the buildings.

Other building styles on the boulevard are representative of restaurants and motels. The Totem Building, which is located on the corner of Ann Morrison Park Drive, provides a rustic Idaho appearance with its log construction, gable roof, and rock fireplace. Another example of an early motel is the Boulevard Motel with its connecting gable fronts accentuated with neon. Both of these buildings are set back from the boulevard. One of the boulevard's most indigenous buildings is the Idaho Forestry Building located adjacent to the Boise River. This rustic log structure was constructed in 1940 to help celebrate 50 years of statehood. The exterior logs are Idaho Engleman spruce. Each interior room features a special wood pattern of a native Idaho timber species. (See page 10.)

Also found along the boulevard are a number of one-story brick buildings remaining from the wholesale activity that took place in the area of Front to Battery streets. Though simple in design, they possess good brick craftsmanship.

Key cultural buildings include the Boise Art Museum, which has been expanded twice from its original 1932 sandstone appearance. Part of the growing cultural center along the boulevard includes the Idaho State Historical Museum and its pioneer village of historic Boise homes and the former brick warehouse that was converted to the Boise Public Library in 1973. Across the river, Boise State University began constructing a collegiate Gothic style building in 1941 with red brick and white terracotta trim. With the exception of the 1942 General Health Building, little campus development took place along the boulevard until 1970 when the circular, twin seven-story tower dormitory was built. This was followed in 1983 with the opening of the 1700-seat Morrison Center for the Performing Arts.

Early contemporary additions to the boulevard have been detrimental to the boulevard's scenic character. The telephone communications building was built on the east side of the boulevard at Bannock Street in the 1950's. This boxy brick building with its rooftop dishes partially obscures the Capitol. In the 1960's the 12-story brick and concrete Bank of Idaho building was constructed on the west side of the boulevard at Idaho Street further blocking the view. This building has recently been renovated into the Key Financial Center, enhancing its ground floor facade to reflect the surrounding historic buildings.

The Boise City Hall, built on Capitol Boulevard in 1978, set a new precedent when the four-story brick building was set back from Capitol Boulevard by 50 feet, creating a plaza space. This entrance space

TABLE 2

**CAPITOL BOULEVARD HISTORIC BUILDING INVENTORY**  
(Buildings located within one block of Capitol Boulevard)

No.	Date	Building	Address
<b><u>BOISE CAPITOL AREA DISTRICT</u></b>			
1	* 1912/1921	Idaho State Capitol	700 Jefferson St.
2	* 1904/1930	Borah Federal Building	304 N. 8th St.
3	* 1910	Idaho Building	216 N. 8th St.
4	* 1908	Fidelity Building	718 Idaho St.
5	* 1908	Union Block	712 Idaho St.
6	* 1926/1978	Egyptian Theater	700 Main St.
<b><u>BOISE HISTORIC DISTRICT (Fronting Capitol)</u></b>			
7	* 1902	Adlemann Block	624 W. Idaho St.
8	* 1879/1927	Perrault Building	625 W. Main St.
9	* 1879	House of Louie	108 S. Capitol Blvd.
10	1914	Anduiza Pelota Court/Brigg's Bldg.	619 Grove St.
11	1930's	Westinghouse Building	315 S. Capitol Blvd.
<b><u>SOUTH EIGHTH STREET DISTRICT</u></b>			
12	* 1906/1910/1981	Falk-Stark Warehouse	400-04 S. 8th St.
13	* 1906/1981	Carlson & Lusk Warehouse	408 S. 8th St.
14	* 1909	Idaho Candy Building	414 S. 8th St.
15	* 1910/1981	Idaho Milling & Elevator Co.	418 S. 8th St.
16	1940's	Idaho Candy Warehouse	500 S. 8th St.
17	1940's/1973	Boise Public Library	715 S. 8th St.
18	1940	Idaho Forestry Building/Log Cabin	801 S. Capitol Blvd.
19	1950/1982	Idaho Historical Museum	610 Julia Davis Dr.
<b><u>Pioneer Village (relocated buildings)</u></b>			
20	1863	Coston Cabin	Julia Davis Park
21	1864	I.B. Pearce Cabin	Julia Davis Park
22	1865	Thomas Logan House	Julia Davis Park
23	c1880	Richard Adelman House	Julia Davis Park
24	1936/1972/1988	Boise Art Museum	670 S. Capitol Blvd.
25	1929	Julia Davis Bandshell	Julia Davis Park
26	1931	Capitol Boulevard Memorial Bridge	Boise River
27	1911	8th Street Bridge	Boise River
28	1942	B.S.U. General Health Bldg.	Diploma St.
29	1938	Boulevard Motel	1121 S. Capitol Blvd.
30	1940's	Totem Building	1301 S. Capitol Blvd.
31	* 1925	Union Pacific Depot	1701 Eastover Terrace

\*National Register of Historic Places

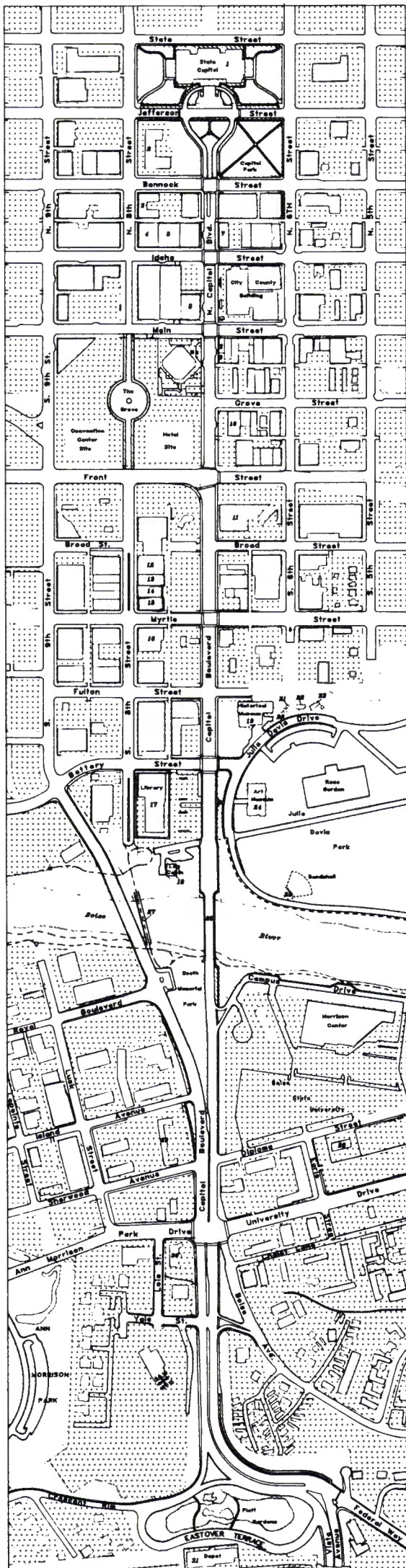


Figure 40. Historic buildings are identified by number

features a memorial and work of art. (See page 12.) The plaza partially covers an underground parking garage, the first built on the boulevard.

Likewise, the Idaho First National Bank's 20-story octagonal shape brick tower, constructed in 1979 as part of the Boise Redevelopment Agency Project, was set back 25 feet from the boulevard. The building's ground level has a landscaped plaza and there is underground parking located off of the boulevard. The building is the tallest in the state, and along with a few other Boise towers to the west, it has established a high rise zone for concentration of the city's skyscrapers. Though a debate continues on the appropriate height for buildings in this area many find the 1988, triangular 11-story First Interstate Bank building to well represent Boise's scale and respect views of the Capitol and foothills.

Summarizing the boulevard's architecture, the majority of buildings typify the progressive and artistic talent of the people of Idaho. But the boulevard's architecture can still be improved. A number of older service stations still exist, reducing the architectural richness of the boulevard. Similarly, a few buildings, such as the Boise Outdoor Advertising building, lack the materials and/or design necessary to complement the boulevard.

Capitol Boulevard's future architecture must reflect the quality and design of the grandeur first envisioned. New architecture will need to communicate a strong image and further enhance the characteristics of this special street.



**Figure 41.** Capitol Boulevard's cultural center in 1950's with original art gallery and historical museum.

## CULTURAL/EDUCATIONAL CENTER

Centered on Capitol Boulevard and adjacent to the Boise River is the city's growing cultural and educational center. The Boise Art Museum, Idaho State Historical Museum, Boise Public Library, and Boise State University are expanding institutions that create a wealth of experiences and activity for the boulevard.

**Boise Art Museum**--Construction of the Boise Art Gallery in Julia Davis Park was begun in 1936 with the Civil Works Administration furnishing the labor for the building and the city providing the materials. The building, which was largely made possible by the Boise Art Society, was constructed of native sandstone with a frontage on Capitol Boulevard. The building, designed by Wayland and Fennell, is a modern Renaissance style and has a concrete roof with skylighting. Architect Jim Fennell had anticipated future expansions and recommended plans for rear wings and a stone sculpture garden. In 1972 the gallery was enlarged with a cement and glass facade. Changing its name to the Boise Art Museum in 1988, an expansion by architects Mark Mack and Steve Trout is an enlarged reinterpretation of the original 1930's building with its restored sandstone entry and its classic symmetry in new materials.

**Idaho Historical Museum**--It took over 10 years for the Idaho State Historical Museum to be built near the entrance to Julia Davis Park. Construction began immediately before World War II with WPA funds and labor but only the basement floor had been completed when war broke out and the WPA was dissolved. In the late 1940's the City of Boise finally asked the State of Idaho to finish the building as it was becoming an eyesore. The building was dedicated in 1950 and is now operated under the auspices of the Idaho State Historical Society with funds from the State of Idaho. The museum, with its growing collection, was expanded in 1983.

**Pioneer Village**--Adjacent to the museum is Pioneer Village, a grouping of historic structures relocated to the park. The Coston and I.B. Pierce cabins, both built in 1863, were relocated to the park where they underwent restoration from 1933 through 1939 in a joint effort by the Sons and Daughters of Idaho Pioneers and the Idaho Statesman. In 1970 the

historic Mayor Thomas Logan house, constructed of adobe, was moved to a place near the cabins. Likewise, the Richard Adelman house was relocated to the village in the early 1980's.

**Boise Public Library**--Opened in a former four-story brick warehouse directly across from the Art Museum in 1973, the Boise Public Library serves as a major catalyst and contributor to the city's cultural center. Room for library expansion exists in upper floors. The library and its parking area have been well landscaped.

**Boise State University**--Idaho's youngest university has rapidly grown to become the state's largest with over 400 full-time faculty, 700 support staff, and a student enrollment that exceeds 11,500.

The 110-acre campus site on the south side of the Boise River between Capitol Boulevard and Broadway Avenue was Boise's airport for nearly twenty years until the city deeded the land for one dollar to Boise Junior College in 1939. Construction of the Administration Building and new campus was initiated in 1940 near the middle of the site. The Administration Building faces the mountains with a formally planned quadrangle to the north. A WPA project helped convert the old airport into a campus with new landscaping, sidewalks, and roads in 1940. Additional buildings of a similar collegiate gothic style were built in 1941 and 1942 of red brick with white terra cotta trim, and designed as a joint venture by Fritz and Frank Hummel of Hummel and Hummel with Wayland and Fennell. Many of the buildings on the campus have followed the model of red brick and white trim exterior set by the Administration Building. Today, there are 46 buildings on the expanding campus.

As illustrated in Figure 42, with the exception of the 1942 General Health Building, little development occurred on the Capitol Boulevard end of the campus until the twin seven-story Towers dormitory was built in 1970. The adjacent Morrison Center for the Performing Arts, designed by Lombard-Conrad Architects, was opened along the Boise River in 1983. This fine hall, along with the B.S.U. pavilion, has had a major impact in involving the community in Boise State University events.



**Figure 42.** Boise Junior College campus in 1940's. Only Health Building (upper left) was located near the boulevard. Notice early boulevard landscaping.



Figure 43. Capitol Boulevard Plan logo.

## PLAN AND ACTION PROGRAM

With the advent of Idaho's 1990 Centennial the time is right to revitalize Capitol Boulevard and realize the city's goals that have long recommended its special treatment. The recommendations offered herein are intended to improve the overall design and management of the boulevard.

## ZONING AND DESIGN REVIEW

Recognizing that Capitol Boulevard is a special place, care must be taken to assure that the boulevard's future development is appropriate and well designed. Under the authority of Sec. 11-2-17.9 Special Design Criteria District of the City of Boise Zoning Ordinance, a Capitol Boulevard Special District is to be adopted by the Boise City Council. The process of adopting the Special Capitol Boulevard District and its guidelines requires the Design Review Committee to recommend them to the Boise City Planning and Zoning Commission for City Council adoptions. To ensure that development is in accordance with the *Capitol Boulevard Plan and Action Program*, design and development guidelines

have been established which set into motion the plan's intent of re-establishing Capitol Boulevard as a grand civic space and making it the focal point of the city's commercial and cultural life.

The district boundaries, which are identified in Figure 2, include the public right of way of Capitol Boulevard and all fronting parcels of land on the east and west sides owned by public and private agencies from the statehouse to the northern edge of the railroad overpass on Vista Avenue. The Capitol Boulevard Special District shall be designated on the zoning maps as *C-2DC*.

For new development to occur, an applicant must get approval from the Design Review Committee prior to the issuance of a building permit. The applicant and architect of any development team is expected to work closely with the planning staff during the design phase of the project. This provides an opportunity for negotiations between the applicant and the review commissions. This system has the capability of effectively regulating public and private development along Capitol Boulevard.



Figure 44. 1988 aerial view of Capitol Boulevard looking south. Photo: C. Bowers.

## LANDSCAPING

The most frequent response from citizens regarding their wishes for the boulevard was the desire to improve landscaping. Therefore, plans call for a public effort to landscape the boulevard within the public right of way and a private effort to introduce landscaping adjacent to the boulevard.

Reinforcing the longitudinal axis of Capitol Boulevard throughout the length of the boulevard is essential. Landscaping would give the boulevard the visual cohesion and comfort it now lacks. Stark contrasts are currently found along the boulevard, ranging from gray parking lots to the lush Boise River area, which overflows with natural vegetation creating a green canopy around the bridge. Likewise, the parks of Capitol, Julia Davis, Booth Memorial, and Platt Gardens provide an inviting oasis from the rather grey boulevard. South of the river a number of small landscaped islands with turf and trees as well as Boise State University frontage soften the boulevard's appearance.

Several attempts to landscape the boulevard in the 1930's and 1960's were frustrated by a lack of adequate irrigation facilities and frequent widening of the highway. A few remaining shrubs and trees planted by the park department and garden clubs in the 1960's can be found south of the river. These include a row of purple flowering plum and golden-rain trees on the west side of the depot approach and a number of ginkgoes and red oaks. As part of the widening of the depot hill in 1985, new landscaping replaced previous efforts and the grassy center median was modified to an evergreen ground cover.

In addition to the visual amenity that landscaping provides, Capitol Boulevard needs to be a comfortable place for the pedestrian. This means providing adequate shading from the hot summer sun as well as maintaining solar access during cold winter days. Planting will also help to screen out dust and exhaust fumes. Likewise, adequate and comfortable seating space is essential for the livability of the boulevard.

*On the north side of the river the boulevard's 80-foot right-of-way calls for a formal tree lined setting leading to the Capitol. Under this plan, a single row of columnar Norway maples and Moraine sweetgums are to be planted along each side. Where room allows, a double row of trees is encouraged. South of the river, red oak and ginkgo trees would be planted the entire length of the boulevard on both sides. This segment will be less formal in its adjoining landscape design due to the traffic configurations of the boulevard and the alignment of South 9th Street and Boise Avenue. Where appropriate, shrubbery would decorate the landscape and innumerable flowers would add rich color. In most cases, turf would be the primary ground cover. It is important to anticipate a sequence of evolving scenes as the trees and plants grow to maturity over the years and the landscape undergoes shifts in scale, light, shadow, and color.*

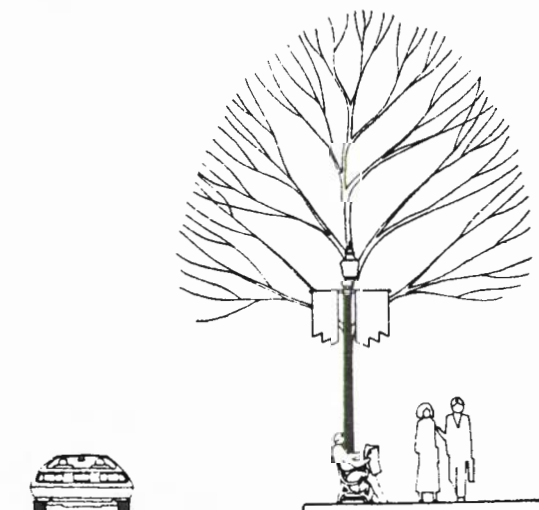


Figure 45. Boulevard Streetscape.

## SIDEWALK IMPROVEMENTS AND UNIFIED STREETScape

More and more residents and visitors will be drawn to the boulevard as major physical improvements are made, transforming it into a lively streetscape and linking a wide variety of cultural, commercial, and governmental attractions. To support this increased pedestrian movement, a number of sidewalk and crossing improvements will be required. Similarly, a streetscape pattern for the boulevard's public edges has been developed to tie its many sub-areas together and provide the visual cohesion it now lacks.

Unified and distinctive streetscape elements of the boulevard include improved sidewalks with a mixture of scored concrete and brick unit pavers along with trees, street lights, and street furniture. Furniture would include litter bins, seating, signage, bus shelters, and planting. Each component of the streetscape is designed to enhance the visual character of the boulevard, and the elements should be consistent the length of the street. Where possible, sidewalks would be widened and separated from the curb. Sidewalks would be transformed into a tree-lined esplanade, giving pedestrians an inviting place to stroll. Where possible, special paving surfaces would connect the sidewalks, emphasizing pedestrian crossings. Not only would the streetscape present a dignified setting for the ceremonial boulevard but it also would provide a functional pedestrian/bicycle system and a vital center of activity for people of the city and state to use, admire, and enjoy.



Figure 46. Battery Street intersection features crosswalks with special paving surface.

In addition to the more formal sidewalk and streetscape treatment from the Boise River north to Bannock Street, a number of sidewalk improvements are proposed for the area south of the river. These include widening and setting the sidewalk away from the curb lane surface on the east side of the boulevard. Designed to accommodate both pedestrians and bicyclists the boulevard's east side sidewalk system would serve as the primary north-south non-motorized link. Plans call for an improved connection and linkage with the greenbelt, establishing a new 12-foot wide sidewalk set back from the boulevard curb along the B.S.U. frontage. Similarly, plans propose setting back a 10-foot wide sidewalk through the Boise Avenue triangular park and connecting with the existing setback sidewalk going up depot hill. The pedestrian/bicycle crossing at University Drive would also be upgraded to better serve these users.

To insure a cohesive identity, certain streetscape features have been adopted for use along the boulevard. Many of these elements are the same as the Elements of Continuity being utilized in the Capital City Development Corporation's (CCDD) downtown project. These elements are identified in Figure 48 and include paving, trees, grates, benches, litter receptacles, moveable planters, drinking fountains, bollards, bicycle racks, and lighting fixtures.

#### LIGHTING

The illumination of Capitol Boulevard will form a continuous row of fixtures, reinforcing the boulevard axis. Public street lights will meet levels for safety and security with roadway intersections requiring higher levels of illumination.

Several varieties of poles and luminaries are installed along the boulevard and a number of Boise historic poles are proposed around Steunenberg Park and in the CCDC area. Table 3 identifies 105 poles, ranging from the single-globe cement standards on the bridge to the contemporary 40-foot black poles with shoe box lights on the depot hill.

TABLE 3

#### CAPITOL BOULEVARD LIGHT STANDARDS

Location	Type	West	East	Total
Capitol Park	Boise Historic*	4	4	8
Bannock to Front	Boise Historic +	16	16	32
Front to River	30' Cast Iron Pole ©	6	8	14
Battery Street	Historic Cast Iron x	1 N.	1 S.	2
Boise River Bridge	Historic Cement *	5	5	10
River to University	30' Cast Iron Pole ©	3	6	9
University to Depot Hill	40' Black	10	8	18
	40' Median Double Arm	—	—	2
Vista to UP Bridge	40' Black	1	1	2
Eastover Terrace	Historic/Octagonal Base	—	—	8
TOTAL				105

\* Public Works to update to Boise historic standard.

+ Cast Iron single-globe Boise historic standard. West side CCDC streetscape. East side proposed.

© Idaho Power Company poles

x Historic lights along Battery and in Julia Davis Park.

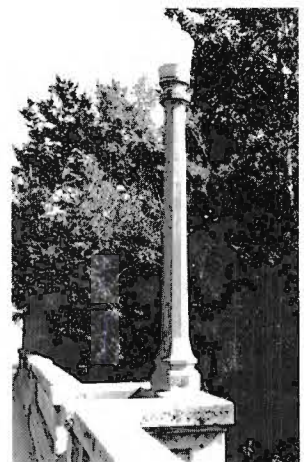
\* Cement single-globe —CB Memorial Bridge 1931 - 4 pylon lights and 4 plaque lights not operable.



Figure 47. 13-foot Boise historic light standard oriented to pedestrians.

Two luminaries have been selected for Capitol Boulevard, serving the needs of pedestrians and drivers. This will allow visual cohesion and optimize maintenance requirements. The Boise historic light standard would be oriented to pedestrians. Similar single-globe lights are currently found at the depot and on the bridge. The historic Boise light prototype is currently planned from Jefferson to Bannock and it is proposed that these lights be extended south to the bridge and located at other special boulevard areas. The poles, which would be painted dark green, would be on a metered system and have a halophane glass globe. The spacing and layout of the lights will be identified by the Boise Public Works Department.

For illumination of intersections and driving surfaces, such as the depot hill approach, simple 40-foot high rectangular-shaped luminaries on a short arched arm, mounted on round tapered steel poles, will be installed. These lights are typically 250-watt high pressure sodium with a shoe box head.



## CAPITOL BOULEVARD FURNITURE

The boulevard's streetscape furniture is identified and illustrated below. Detailed specifications, dimensions, and color selections are contained in other documents, which should be consulted prior to specifying materials and design features.

**Pavers** are to be of standard dimension--4 by 8 rectangular, clay units. Handtight mortarless joints laid on sand or placed on concrete where subjected to vehicular traffic. Patterns may vary. (See depot platform.)

**Concrete.** Poured in place sidewalks shall have a 24- or 36-inch square scoring pattern. Begin score pattern from back of curb and place paper expansion joints a maximum of 50 feet apart and at the curb and building or property line.

**Benches** can be located in various configurations and should be be cast iron with wooden slats (model "B-76"), as manufactured by Bench Manufacturing Company, Boston, Massachusetts.

**Drinking Fountains** should be model 1896 cast metal as manufactured by Canterbury International, Sherman Oaks, California.

**Bollards** should be used to define special areas and to enhance pedestrian safety at vehicular crossings. Manufactured by Canterbury International.

**Tree Grates** should be "Standard Flat" four-foot square, cast iron as manufactured as "Eco Grates" by Urban Accessories, Snohomish, Washington.

**Tree Guards** should be limited in use to areas where pedestrian activity might result in damage to the trees.

Manufactured as "Eco Guard" by Urban Accessories.

**Trench Grates** may be used under certain conditions.

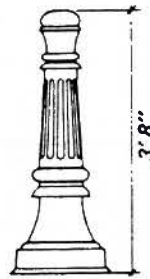
These grates should be cast iron in the "wave" pattern. Manufactured by Urban Accessories.

**Moveable Planters** should be cast stone as manufactured by DuraArt Stone, Newark, California.

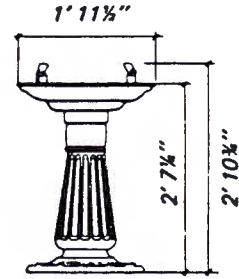
**Litter Receptacles** should be cast stone with metal cover, as manufactured by DuraArt Stone.

**Bicycle Racks** should be model "C1 Bike Bollard," as manufactured by Urban Accessories.

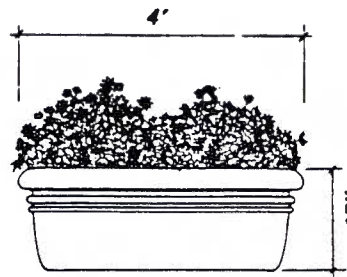
**Newspaper Dispensers.** A metal two dispenser pedestal-type unit should be used. "Sho-Rack model K-4916" or "TK-4916" manufactured by Kasper Wire Works, Shiner, Texas.



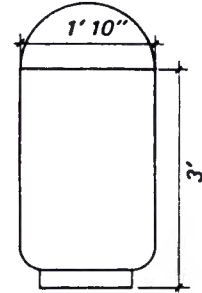
Bollard, elevation



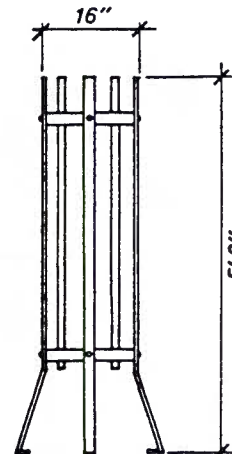
Drinking fountain



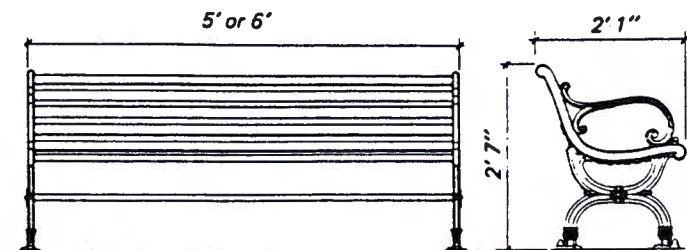
Movable planter, elevation



Litter receptacle

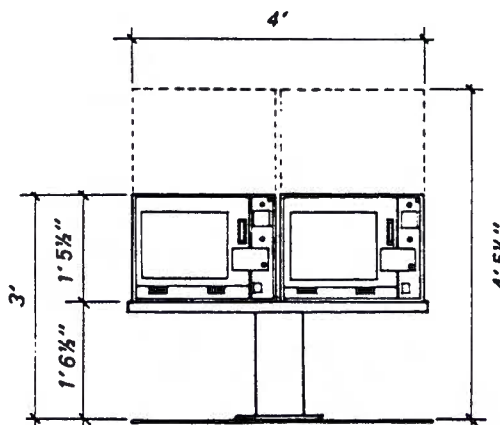


Tree guard, elevation

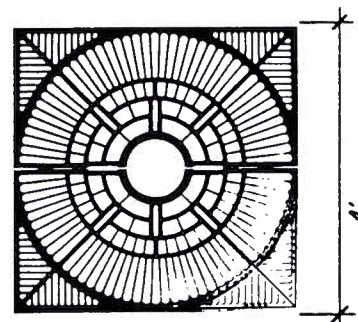


Bench, front elevation

Bench, side elevation



Newspaper dispensers, front elevation



tree grate, plan

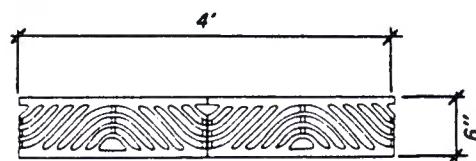
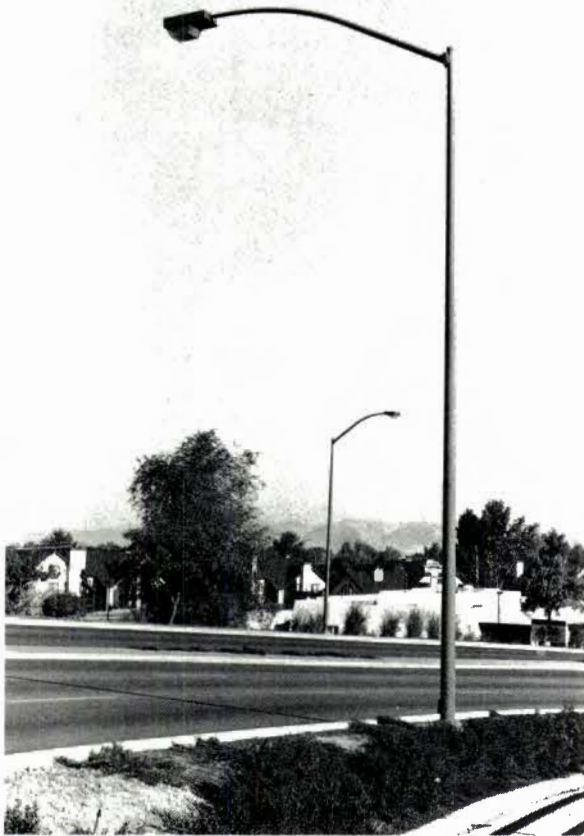
Trench grate, plan  
(Radius sections available)

Figure 48. Streetscape Furniture.



**Figure 49.** 40-foot rectangular shaped luminary on short arched arm for lighting driving surfaces and intersections.

#### Traffic Control Signals

Traffic control signals are located at eight intersections along Capitol Boulevard. These cross streets are Bannock, Idaho, Main, Front, Myrtle, Battery, University, and the Federal Way/Vista intersection. The signals are similar to, but shorter than, the 40-foot high tapered light fixture poles being used on the boulevard. Bolted to the pavement the signal poles are adaptable to various intersection configurations, typically having three poles at each intersection plus a pedestrian pole.

To visually simplify each intersection, pedestrian signals, lighting, and signage are combined with the traffic signal supports. The mast arms are cantilevered over Capitol Boulevard and have varying lengths and a number of signals. It is important to point out that whenever possible the length of the mast arms and number of signals should be kept to a minimum to preserve the views of the Capitol and depot. Similarly, an earlier type signal structure at University Drive is overly massive and should be redesigned. All metal hardware should be painted the standard black of street furniture.

#### Building Lighting

Where possible, individual buildings, monuments, and major landscaped areas would be specially lit to make them stand out from their surroundings.

The Capitol dome has been faithfully lit for years, inspiring the city with its evening focal point. Individual bulb lights first adorned the Capitol for the

inauguration of Governor John Haines in 1913. In October 1953, the dome of Idaho's statehouse took on a radiant glow, bathing the masonry in the soft glow that we look upon today. J.E. Tourtellotte, the architect of the Capitol, often spoke of its integral ornamental lighting system that would "outline the angles, cornices, and constructural portions of the building, giving the building, at night when illuminated, an even more beautiful appearance that it has at noonday." Similarly, the Union Pacific Depot tower was lit until recently.

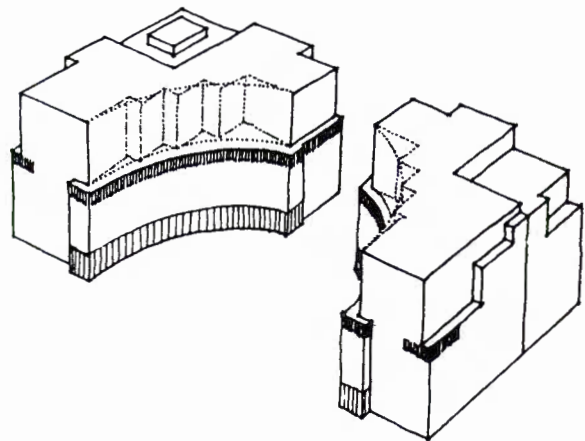
#### DEVELOPMENT PLAN

The land uses proposed in the plan include those uses currently found along Capitol Boulevard and new uses that are intended to create continued activity for the area. A mixed use development approach is encouraged, including retail, office, and residential. The liveliest possible mixture of commercial uses will be encouraged, including restaurants, theaters, night clubs, art galleries, boutiques, and specialty shops that naturally attract a large number of users and stimulate street life.

It is the recommendation of the *Capitol Boulevard Plan and Action Program* to provide a combination of old and new, retaining all designated historic structures. The special characteristics and scale of the boulevard's historic districts would also be safeguarded. The boulevard will maintain a sense of historic continuity and evolution by preserving buildings representative of different eras and styles. This tangible evidence of the boulevard's development and change in use and scale will provide a link with the past.

The boulevard continues to encourage residential development, including the development of a new hotel and expansion of the motel district. Likewise, the introduction of new residential housing and residents will help to make the area lively, attractive, and safe.

It is in the city's interest to require that the land adjacent to Capitol Boulevard be developed and used in a manner suitable to its ceremonial, physical, and historical relationships. For example, the boulevard is a most promising site for buildings of state or cultural importance.



**Figure 50.** Concept of new development reflecting classical architecture and stepped back design of upper floor.

A number of development opportunities occur on vacant or currently underutilized land. The boulevard's Special District requires that new buildings be representative of the best contemporary architectural and planning concepts.

Design criteria have been established for the boulevard to maintain setbacks and light and ensure designs that retain views of the Capitol and depot. Likewise, height and bulk controls have been established in response to tall buildings that have diminished the boulevard's scale and character. New buildings that flank the boulevard are encouraged to respect the boulevard's classical development with special emphasis on the setback, height, architectural character, and use.

The boulevard itself would be enhanced both by special landscaping and lighting. Continuing demand for new and upgraded commercial enterprises will come from the large downtown employment base and augmented by increased numbers of both tourists and local residents.

**Design Guidelines for New Development**

New buildings should be considered elements in a classical and urban composition that centers on the Capitol and depot as focal points.

Development should exhibit the best of contemporary architecture and planning with special care taken to integrate this development with the boulevard.

The City of Boise reserves the right to determine whether a proposed building design achieves an acceptable level of coordination with the boulevard.

To preserve the longitudinal visual integrity of the boulevard a 20-foot building setback from the property line along Capitol Boulevard is required. The landscaped setback area may include an expanded sidewalk, plaza, architectural gateway, entrance foyer, and limited retail space. Buildings within the Old Boise Historic District are exempt from the setback requirement.

For buildings over two stories (25 feet total) the building shall be stepped back at the third story an additional 15 feet for a total distance of 35 feet from the boulevard property line.

Visual and symbolic significance of the scenic corridor of the Capitol Dome shall be protected by restricting any new building from dominating the structure.

Future standards to protect light, air, and solar access at the street level along the boulevard should be established.

The upper portion of any building should be treated in a manner that creates a visually distinctive roof or other termination of the building facade. Similarly, where possible, such buildings heights should step

down gradually to equal that of existing buildings on the boulevard.

Construction materials (i.e. stone, brick, glass) shall be selected that reflect the quality of Capitol Boulevard and achieve a harmony with the building and adjoining buildings.

New development should respect the boulevard's streetscape and enhance it by adding new vitality.

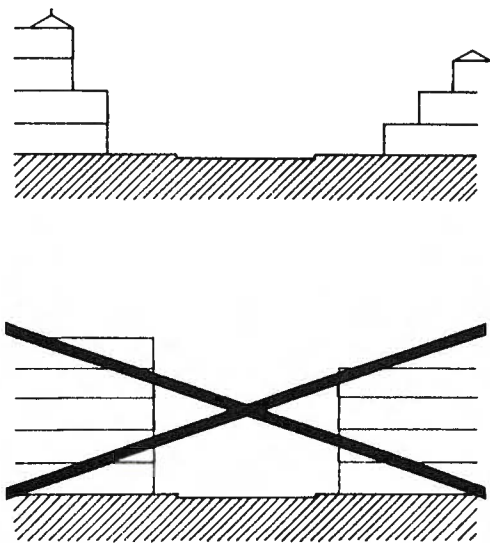
All development projects facing the boulevard will be required to follow a uniform design for public streetscape improvements, including sidewalk, pavings, street trees, and street furnishings. Sidewalks on all side streets shall be developed in a consistent manner.

Integrate the architectural form and the open space around a new building to enhance the quality of the pedestrian environment. Pedestrian features at the ground level of development, including weather protection, sidewalk paving, landscaping, street furniture, signage, lighting, building entrances, and lobbies shall be attractively and appropriately designed.

All mechanical equipment shall be screened. All utilities shall be placed underground.

A strip of land abutting the Capitol Boulevard right-of-way shall be landscaped with shrubs, trees, and ground cover. The planting strip of land shall have a minimum depth of ten feet and may include an expanded sidewalk area. (See Table 1.)

Landscape treatment shall be provided to enhance the building's architectural features and strengthen the boulevard's vistas.



**Figure 51.** Sections through Capitol Boulevard, delineating the concept of development being setback from the property line and maintaining the scenic corridor to the Capitol and depot.

## SIGNS

One of the major concerns expressed by the public in the Capitol Boulevard questionnaire was the need to clean up both private and public signs along the boulevard. Although the visual environment is one of the boulevard's best features it is sorely lacking in visual organization. Looking at the boulevard as a scenic corridor within a special design district, new sign criteria and regulations have been established to safeguard the boulevard's aesthetic qualities. Private business identification signs need to become a part of the architecture of the building rather than being an unrelated object. Flush-mounted wall signs parallel to the boulevard with individual letters are encouraged. Small low profile monument-style signs integrated with the building and landscaping may also be utilized in lieu of wall signs. Pole mounted signs, which conflict with the view of Capitol Boulevard, are prohibited.

With the boulevard taking on a first-class address, property and business owners along the boulevard will be encouraged to conform and comply with the new guidelines. Non-conforming signs along the boulevard will be identified with an amortization schedule that will allow the sign owner to recoup their investment during a period of 2 to 7 years.

Already a number of sign changes are improving the visual appearance of the boulevard, such as the removal of the large First Interstate signs from the recently renovated Key Financial Center building. Likewise, public signing will become uniform in size and color and better assist the traveling public in directing them to the boulevard's many features.



**Figure 52.** Pole mounted signs and reader boards would be prohibited on the boulevard.

## Private Sign Regulations

Recognizing that Capitol Boulevard is the ceremonial gateway to Boise and the State of Idaho the scenic vistas of the Capitol and Union Pacific Railroad Depot must be preserved and enhanced. Signs located within the Capitol Boulevard Special District fall under specific sign regulations to preserve the distinctive appearance and reinforce the character of the boulevard in its natural setting. (See map on page 2.)

Sign standards shall be uniformly applied to promote fairness in competition and retain identity within the business community. Providing equitable standards to all businesses along Capitol Boulevard allows each individual business to clearly identify itself and the goods and services that are offered, as well as to maintain the high aesthetic values that both residents and visitors desire for the grand approach and scenic corridor to Idaho's seat of government.

Signs on Capitol Boulevard are regulated through the City of Boise's Design Review Process and require a permit. The following criteria is utilized to regulate and control the design, quality of materials, and the construction, illumination, location, and maintenance of all signs, sign structures, and billboards. The criteria for signs on Capitol Boulevard include the following:

### Design Guidelines for Signs:

One main identification sign shall be allowed for each separate business.

The main sign must be located adjacent to or on the building facing or parallel to Capitol Boulevard. An identification sign shall not exceed 100 square feet in area. For a building set back 35 feet or more from the boulevard property line the identification sign shall not exceed 150 square feet in area.

Signs are to be integrated with and harmonious to the building and site.

Individual letters are encouraged over a box panel sign.

In the instance of a multiple tenancy building, each individual company may have a wall sign over the entrance to identify the business. Such signs shall be of uniform size, design, and height and shall be compatible throughout.

The top of any sign shall not be higher than the building on which it is located and not higher than 28 feet.

Reader boards or rotating or flashing signs shall not be permitted along Capitol Boulevard nor any other prohibited signs as outlined under Sec. 11-3-4.6 of the Boise City Code.

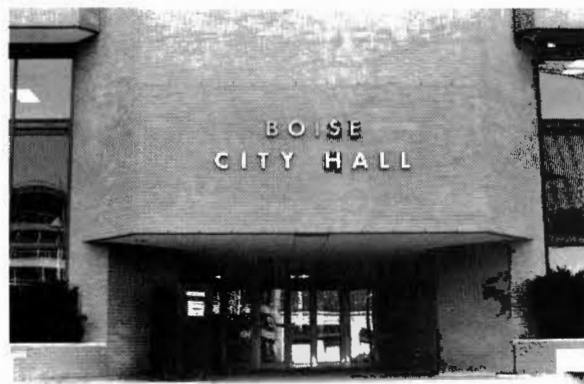
Freestanding pole signs shall be prohibited.

A low-profile freestanding sign (monument signs) may be authorized if the sign incorporates the design and the building materials accentuate the architectural theme of the building on the same property. Signs would be limited to two faces and a maximum height of five feet with a maximum of 35 square feet per face. Height is measured from the top of the street curb to the highest projection of the sign structure. If a monument sign is utilized it shall be in lieu of other identification signs. A landscaped area shall be provided on site at the Capitol Boulevard frontage at the base of the monument sign, with landscaping area to have a minimum of four square feet for each square foot of sign area.

Price signs are allowed for service stations only, and the maximum area shall not exceed 12 square feet and shall be counted in the total aggregate sign area.

Off-premise signs, billboards, and bulletin boards shall not be permitted along the boulevard, nor within one block in all directions of the boulevard.

The above specific sign controls shall not apply to the marquee of the Egyptian Theater nor to the Boulevard Motel's main sign due to their historic status.



**Figure 53.** Flush mounted wall signs mounted parallel to the boulevard and using individual letters are encouraged. Most of the boulevard's public buildings are identified in brass letters.

### Amortization of Non-Conforming Signs

All signs rendered non-conforming by virtue of the provision of the Capitol Boulevard Special District Guidelines shall be amortized within a period of seven (7) years. By 1996 such signs shall be either removed or brought up to the criteria requirements within the period of time prescribed herein defined from the adoption of these guidelines. An amortization schedule based on sign values over seven years will be prepared. The planning director upon a determination that a sign is in violation shall give notice to the owner to remove or alter the non-conforming sign.

### Public Signs

Various guide signs are required within the Capitol Boulevard right-of-way to safely direct traffic and assist visitors and local residents in finding points of interest and urban institutions. Not only do these guide signs need to be easily read and uniform in design but they must blend with the aesthetic character of the boulevard. Guide signs on Capitol Boulevard should be kept to a minimum and only installed where necessary for direction. Currently, many of the existing guide signs detract from the Capitol Boulevard vista and obscure important views. Other guide signs are needed to identify the boulevard's cultural and historic sites.

Guide signs (i.e. Boise Avenue, University Drive) should not exceed a maximum size of 3' by 3' (9 sq. ft.). Such signs would have a white reflectorized legend (6" letters) on a green background. The large overhead freeway-type sign that spans Capitol Boulevard near Broad Street should be removed once the Boise Connector is complete. The similar overhead signs spanning the southbound lanes of Capitol Boulevard near Island Street and Capitol Boulevard on the depot hill should be modified to lessen its freeway image. Guide signs identified for the boulevard include those for Amtrak, Boise Avenue, University Drive, Boise River and Greenbelt, Tourist Information (white on blue), I-184 routemarker, and signs for cultural and historic places.

Guide signs are recommended to identify cultural and historical facilities located on or adjacent to Capitol Boulevard. Such signs would have a white reflectorized legend (6" letters and directional arrow) on a brown background. The maximum size of such signs would be 3' by 3'. Directional guide signs identified for the boulevard include those for Ann Morrison Park, Boise State University, Morrison Center, the zoo and museums, Julia Davis Park -right lane, South 8th Street Historic District, Basque Museum, Old Boise Historic District, and The Grove.

In coordination with the guide signs it is also recommended that new entrance or site signs be installed to identify the Platt Gardens, Ann Morrison Park, Boise State University, Julia Davis Park, and Capitol Park. The park department's stone and wood prototype sign would be appropriate.

## MANAGEMENT

Management of the boulevard falls under the jurisdiction of a number of agencies. Because of this multi-jurisdictional control a high degree of coordination is required between the various agencies to fulfill the boulevard's potential. These agencies include the Ada County Highway District, which controls the boulevard's right of way and its traffic and transportation functions; the Boise City Community Planning and Development Department that oversees the design and zoning functions of the boulevard and approves any future building modifications; and the Boise City Parks Department, which cares for and maintains the six parks and landscaped medians and islands along Capitol Boulevard. Another agency, the Boise City Public Works Department, supervises the boulevard's light fixtures.

Public projects identified in this plan call for specific Capitol Boulevard reconstruction projects (i.e. street-scape rebuilding with improved sidewalks, trees, and lighting) as well as long-term maintenance.

The Boise City Parks Department takes on the continuing responsibility of fostering and maintaining the various parks and green spaces along the boulevard. Realizing that routine maintenance in the past has been inadequate, the department would take on an increased responsibility for monitoring and maintaining the boulevard. Increased revenues sufficient to provide a higher standard of performance are identified on page 49. Throughout the boulevard, park work would include shrub and tree planting, pruning, turf care, painting, minor repairs and removal of garbage. Also, a variety of specialized tasks, such as graffiti removal, small scale restoration, and preventive maintenance of the artistic and historic components of the boulevard's monuments, arbors, etc. should occur. The primary ongoing function, though, is groundskeeping, including planting, pruning, mowing, mulching, fertilizing, and watering the boulevard's lawns, flowers, shrubs, ground cover, and trees.

With the park department's office located at the northeast entrance to Ann Morrison Park, they are well situated to monitor and direct the care of Capitol Boulevard through the department's superintendent, staff, and crew.

The Ada County Highway District shares management responsibilities in improving and maintaining the boulevard's traffic functions, including bicycle and pedestrian systems and public guide signs.

To assist the various agencies in revitalizing the boulevard, a Friends of Capitol Boulevard Committee would be formed. The organization's primary mission is to locate private funding for the endowment and to back innovative programs and projects that cannot be done with city funds. Another of its functions is to provide guardianship to Capitol Boulevard to help ensure that it never again becomes poorly maintained and managed with indifference.

The Friends of Capitol Boulevard will be incorporated as a non-profit tax-exempt corporation (503-C3) and run by a Board of Directors that represents the existing AIA Committee and a broad segment of the community. The endowment would be overseen by the group in close coordination with the Boise City Parks Department.

## BOULEVARD EDUCATION AND PAGEANTRY

Experiencing and interpreting Capitol Boulevard by all age groups is a part of a continuing educational program planned for the boulevard. The boulevard can serve as an opportunity for learning about Boise's history and its changing character. Not only is it rich in instructional value but its cultural qualities provide entertainment and its visual and natural beauty can be emotionally uplifting.

Reference materials (i.e. maps, brochures, guide books, and interpretive signs) need to be prepared to help people understand the boulevard's history and features. Also, a new walking tour for the boulevard is proposed. Such a tour would link the Capitol, Capitol Park, downtown, Old Boise, 8th Street Warehouse District, Julia Davis Park, the Boise River Greenbelt, B.S.U., and the depot. It would include a monument trail identifying the boulevard's many memorials and works of art. A seasonal calendar of events is also proposed due to the many activities taking place on the boulevard. Interpretive signs are proposed to tell the story of Capitol Boulevard, including Capitol Park, Boise's former Chinatown, Old Boise, the Basque neighborhood, and the former Barber Spur just south of Front Street. Another sign will tell the history of the ferry and various bridges that cross the Boise River, and there will be another for the Platt Gardens and the depot. Various architectural drawings (i.e. the elevation of the Capitol) and works of art, such as those depicting Capitol Boulevard, the bridge, and the depot, would be made into posters and note cards to promote the boulevard. These various educational and promotional materials would be accomplished through the assistance of the Friends of Capitol Boulevard.



**Figure 54.** Children take over the boulevard in the annual Capitol Classic.



**Figure 55.** Capitol Classic logo.

Another part of promoting the boulevard includes celebrations, festivals, parades, fun runs, and similar events. A number of fun runs currently utilize the boulevard, such as the Capitol Classic, which has given many of Boise's children the opportunity to enjoy the boulevard while it is closed to traffic. Sponsored by the St. Alphonsus Regional Medical Center, kids of all ages and abilities race to the depot, promoting healthful activities for children. All participants receive a gold medal, t-shirt, and refreshments at Platt Gardens upon conclusion of the race.

The largest event that impacts the boulevard is the annual Art in the Park, which is sponsored by the Boise Art Museum in Julia Davis Park. Now in its 36th year, the festival is a major show place for over 200 of the region's leading artists and craftsmen, providing them an opportunity to show and sell their wares. Supplemented by international food concessions and performing arts, the mid-September event draws large crowds that spill out onto Capitol Boulevard. Other events, such as the downtown's Streets for People, utilize the boulevard and Capitol Park. The revival of the Soap Box Derby, along with new events and future parades, is encouraged to make full use of the boulevard's various features and parks. A Capitol Boulevard Business Improvement District (BID) will utilize some of the revenue generated by boulevard businesses to promote and enhance the street. (See page 49.)



**Figure 56.** 1925 and 1955 brochures featuring Boise's highlights. A new boulevard brochure is proposed.



**Figure 57.** Capitol Classic finishing at Platt Gardens.

While twenty-one flagpoles currently grace the boulevard, many are not utilized. These flagpoles should be a continuing part of the boulevard's pageantry, flying the U.S. flag, the Idaho State flag, and various logos or foreign flags.

Boise's mounted police should be utilized on Capitol Boulevard and its various parks for special events. The mounted police are more accessible and give a better sense of protection to pedestrians than do police cruising in cars.

#### County Seal Shields

Another project planned for the centennial celebration will honor Idaho's 44 counties by painting the county seals on 24" round metal shields and attaching them to 44 light fixtures on Capitol Boulevard. The seals, which will be enjoyed by pedestrians and motorists alike, have a long lasting baked enamel finish. The project has been graciously funded by Albertsons Inc. and will be unveiled for Idaho's Centennial. (See the prototype in Figure 80.)

#### Cloth Banners

Other opportunities exist to celebrate the boulevard and city, including flying decorative cloth banners from light standards. Mounted on both the historic and modern 40-foot light standards along Capitol Boulevard, these banners would visually enhance the grand entrance by adding a colorful graphic design(s) representing Boise and a variety of changing images. The banners would be made of an ultraviolet-treated awning fabric (two-year life expectancy) with hemmed edges and brass grommets for fastening and would be imprinted with up to four colors. The banners would be mounted with a pole clamp bracket and uniformly placed on the historic standards or 18 feet above sidewalk grade for the 40-foot standards.

BOULEVARD PROJECTS

Various sections of the boulevard present different opportunities and problems. A close examination of these areas has resulted in the following development plan of specific proposals for individual areas. The recommendations indicate the thrust of the proposed work but, of necessity, do not always include specific details. These will come later as the projects are refined and as work on the boulevard goes forward.

CAPITOL PARK-CENTENNIAL CELEBRATION

Built in 1920, Capitol Park serves as the grand approach to the state's Capitol. Featuring a formal Y-shape roadway entrance, this one and one-half block park contains an expanse of grass and mature trees that compliment the Capitol grounds. The park has proven its value as an urban space and is well used by pedestrians and lunch time picnickers. (See the park's history on pages 6 and 18.)

To commemorate Idaho's centennial it is proposed that the park undergo a number of improvements. A new bronze-cast sculpture Of Idaho Roots by artist Nancy Draher will be unveiled in 1990. Another recommendation is to develop a plaza and water feature in the north center of the park. Designed for an assemblage of people, the space would contain a number of commemorative plaques that would identify events, achievements, or contributions. The plaza would have a sandstone water feature to mask external sounds and provide a new attraction. Other components include benches, new plantings, Boise historic lights, a drinking fountain, new park signs, and a stage area. Pedestrian movement to and within Capitol Park would be improved with an expanded pedestrian path network that crosses the park, connecting the park's three segments together along with the Capitol grounds. Street crosswalks for the park on Capitol and Jefferson would be of a distinctive paving material. The park's existing shade trees would be retained.



Figure 59. Aerial view of Capitol Park and Capitol grounds.

Other improvements call for additional short-term, on-street parking for the park and the Capitol and a visitor information kiosk that would be tied to the Boise Chamber of Commerce office located across from the park on 6th Street. Improvements for picnickers would also be made to the portion of the park behind the old Federal Building. Another minor change is the trimming back of much of the landscaping around the Steunenberg Monument to better expose its features and central location. Plans are already underway by the Boise City Public Works Department and Idaho Power Company to replace the light fixtures around the monument's roadway with the Boise historic lights that originally graced the site. Further consideration should be given to closing the roadway to cars. This would tie the three park segments together but would disrupt the historic access to the Capitol. All modifications to the park should be in keeping with the stature of the Capitol.

Refinements of this concept plan would be made by the Idaho Chapter of the American Society of Landscape Architects. The Boise City Parks Department will oversee the park's construction and provide continuing maintenance.

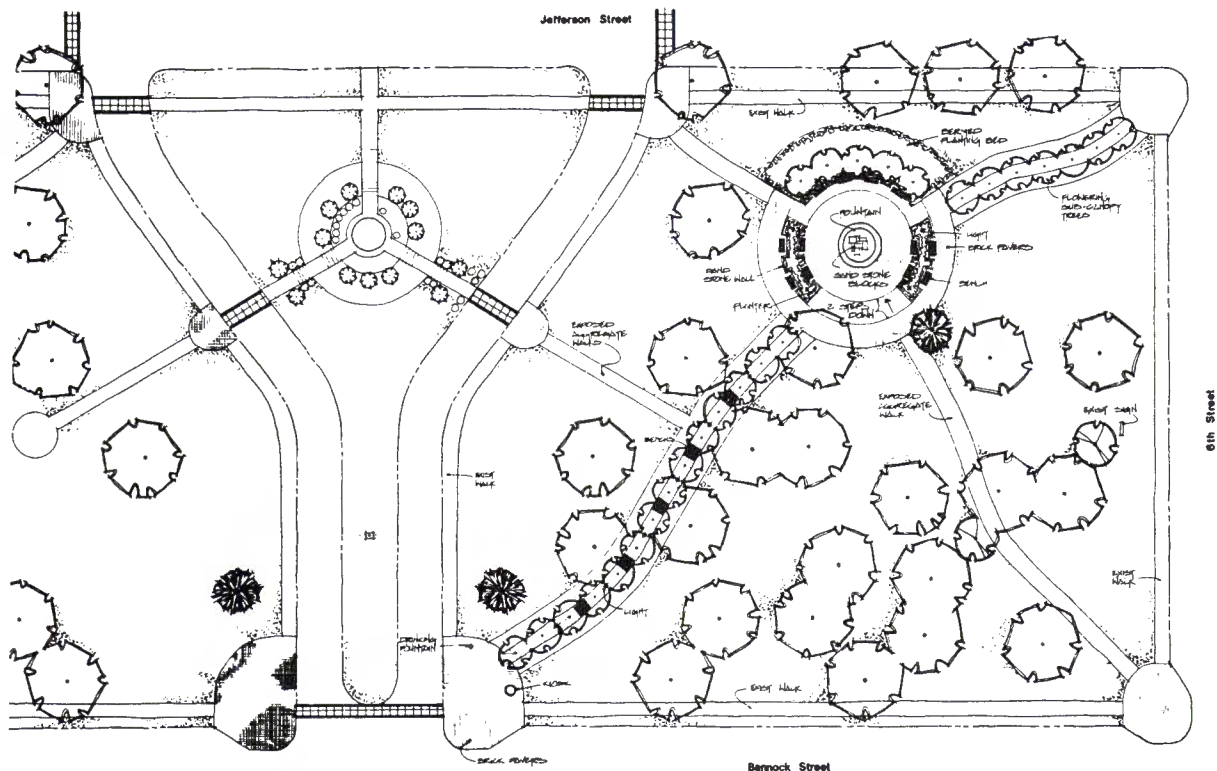


Figure 58 Capitol Park Concept Plan.

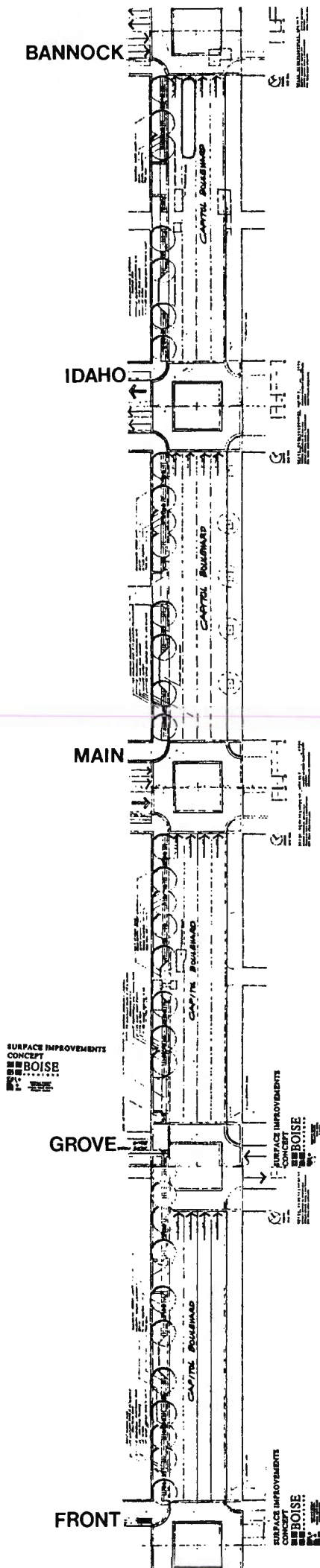


Figure 60. Bannock to Front Street--streetscape prototype.

### BANNOCK TO FRONT STREETSCAPE

An established streetscape program is underway on this four-block downtown segment of the boulevard. As illustrated in Figure 60, the Capital City Development Corporation has adopted a streetscape plan for the west side of Capitol Boulevard. The improvements include brick sidewalks, columnar Norway maple trees and the Boise historic street lights. Streetscape furniture, such as benches, litter bins, and tree grates, have also been identified in the CCDC's Elements of Continuity Report. The street furniture and many of these same elements will also be utilized on Capitol Boulevard. (See page 30.)

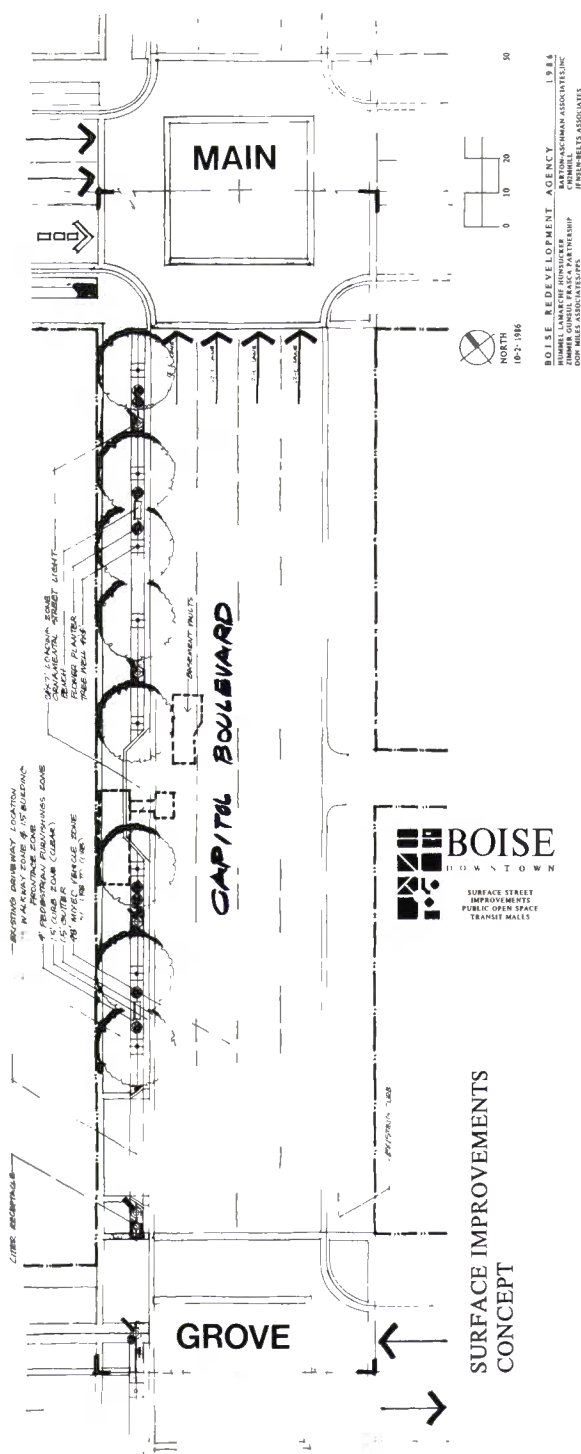
The boulevard's roadbed is new, including the curb and gutter, which was completely rebuilt in 1988 by the Ada County Highway District. The roadway was narrowed slightly, providing a 52-foot wide traffic surface with 14-foot sidewalks on both sides. The four-block alignment of the new curb lines are in most cases uniform and feature one-vehicle loading-unloading pullouts on most of the fronting blocks. Another boulevard modification was the addition of a small traffic median near Bannock Street. This median will be landscaped with turf and flowers similar to the Capitol Park median directly to the north, helping to maximize the visual approach to the state's Capitol.

Completion of the streetscape area between the finished curb to the adjacent property line will be completed in phases. Starting with the recently renovated Key Financial Center, the west side of the boulevard will be completed by the CCDC.

With years of downtown redevelopment now coming to fruition it is important to complete this segment of Capitol Boulevard by Idaho's 1990 Centennial. Therefore, the plan calls for the east side streetscape to mirror the west side and provide uniformity to the boulevard with similar brick paving, trees, and lights. It is important that both sides of the boulevard's streetscape present a uniform appearance and that trees be planted at the same time to maintain a consistent growing pattern. A blend of public and private funds are to be utilized to complete the streetscape. East side property owners include AT&T, U.S. West Communications, Ben Fong, Boise City (City Hall), Harry Fritchman, EBCO Inc., Archibald Steel, and Charles Schmoeger. It would also be appropriate for the CCDC to participate in the east side of the boulevard's streetscape improvements as well as those planned for the west side.



**Figure 61.** Handsome brick pavers similar to those at the depot being set into place for the Key Financial Center block.



**Figure 62.** Details of the Capitol Boulevard streetscape prototype from Main to Grove streets. Both the east and west streetscape will be improved with similar brick paving, street trees, and Boise historic lights.

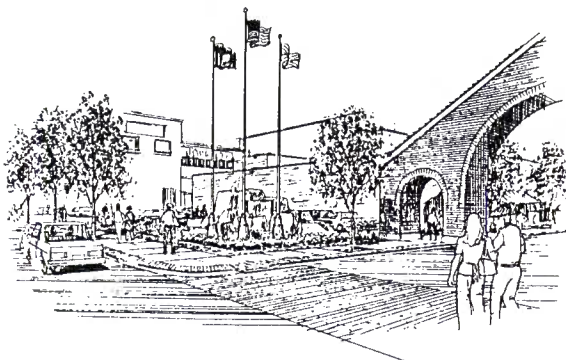
## OLD BOISE

Other than the streetscape project along Capitol Boulevard and upgrading the alleyways into pedestrian spines, few changes are called for in the Old Boise District with the exception of the Basque neighborhood. As part of the Capitol streetscape project identified earlier, funds have been solicited to install three interpretive plaques along this section of the boulevard to commemorate Boise's Chinatown, the Old Boise Historic District, and the Basque Neighborhood.

## THE BASQUE NEIGHBORHOOD MARKETPLACE

Centered off Grove Street, the Basque Neighborhood Marketplace offers a unique cultural opportunity for the boulevard. Plans for the area propose the development of a Basque cultural center that utilizes rehabilitated buildings, including the Basque Center, Basque Museum, and the Anduiza Pelota Court, along with a new streetscape program for Grove Street.

The private lots facing Capitol Boulevard in this one and one-half block area are in need of attention. They currently serve as parking lots and have no landscaping, creating a negative entrance to the Basque area. This can be overcome by redesigning the edges of this needed parking for the Basque neighborhood. The proposed boulevard streetscape for the area calls for columnar Norway maples and Boise historic light standards. In addition to these elements, the streetscape in this area should reflect the history and ethnic flavor of the Basque neighborhood. A stucco wall featuring tile, wrought iron, and sandstone trim is recommended to define the street edge. Additional attention would be focused on the east side of the Grove Street and Capitol Boulevard intersection, which would be highlighted by an arched entrance, flagpoles, a water amenity, and landscaping. For further information see the Basque Neighborhood Marketplace feasibility study.



**Figure 63.** Capitol and Grove intersection, featuring a concept drawing of arch, flagpoles, and landscaping.

## FRONT STREET TO BOISE RIVER

This five and one-half block segment is well established at its southern end with Julia Davis Park and the Boise River forming a cultural and recreational center; however, the remaining blocks are poorly defined with much of the frontage devoted to parking lots.

A new streetscape is proposed for the entire length, creating a uniform, formal approach adjacent to the ten-foot sidewalk areas, and is illustrated in Figure 64. This streetscape is similar to that under construction from Front to Bannock streets, and will contain Moraine sweetgum trees, Boise historic light fixtures, and scored concrete and brick sidewalk paving. Street furniture, such as benches, tree grates, and planters, are also proposed and are identified on page 30. As all existing buildings within this segment have setbacks of ten or more feet from the boulevard right-of-way, additional opportunities for expanded landscaping and a wider sidewalk are proposed. (See pages 9 and 23).

The three-block area fronting Capitol Boulevard from Front Street to Fulton Street presents numerous opportunities for private redevelopment. This area contains a number of private, vacant parcels that are utilized for surface parking. The area and its future development will also be impacted by major east-west Broadway-Chinden traffic on Myrtle and Front streets.

Key corner properties are available, straddling Capitol Boulevard on the former Barber Rail Spur, and could be incorporated into the 8th Street Marketplace area on the west and into the Westinghouse Building (#11 Historic Building Inventory) rehabilitation or redevelopment on the east. A footnote to

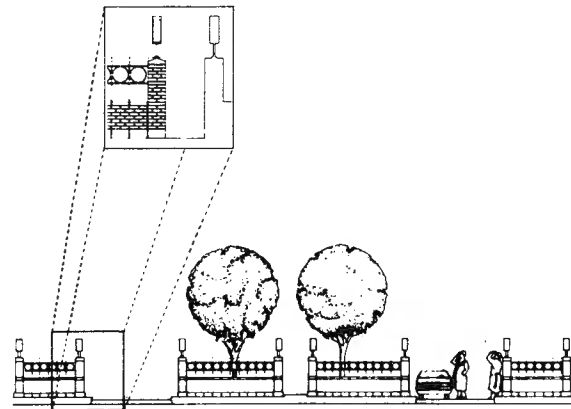


Figure 65. Example of street wall to screen boulevard parking lots.

this area includes the installation of an interpretive plaque at the Capitol Boulevard and Barber Rail Spur location to commemorate the former rail line's role in the development of Boise. The opportunity also exists to close Broad Street on both sides of Capitol Boulevard to create larger development parcels. Development guidelines in this area require a 20-foot setback from the boulevard right-of-way and a 35-foot setback for the third and additional stories. (See page 32.)

Streetscape improvements in this segment would occur when new private development or remodeling takes place. An effort is also being made to locate federal or tax increment financing to assist property owners in accomplishing the streetscape improvements. As an interim effort, the existing light poles would be repainted by the Boise Public Works Department through the authorization of Idaho Power. A new well is also needed north of the river to irrigate the street trees.

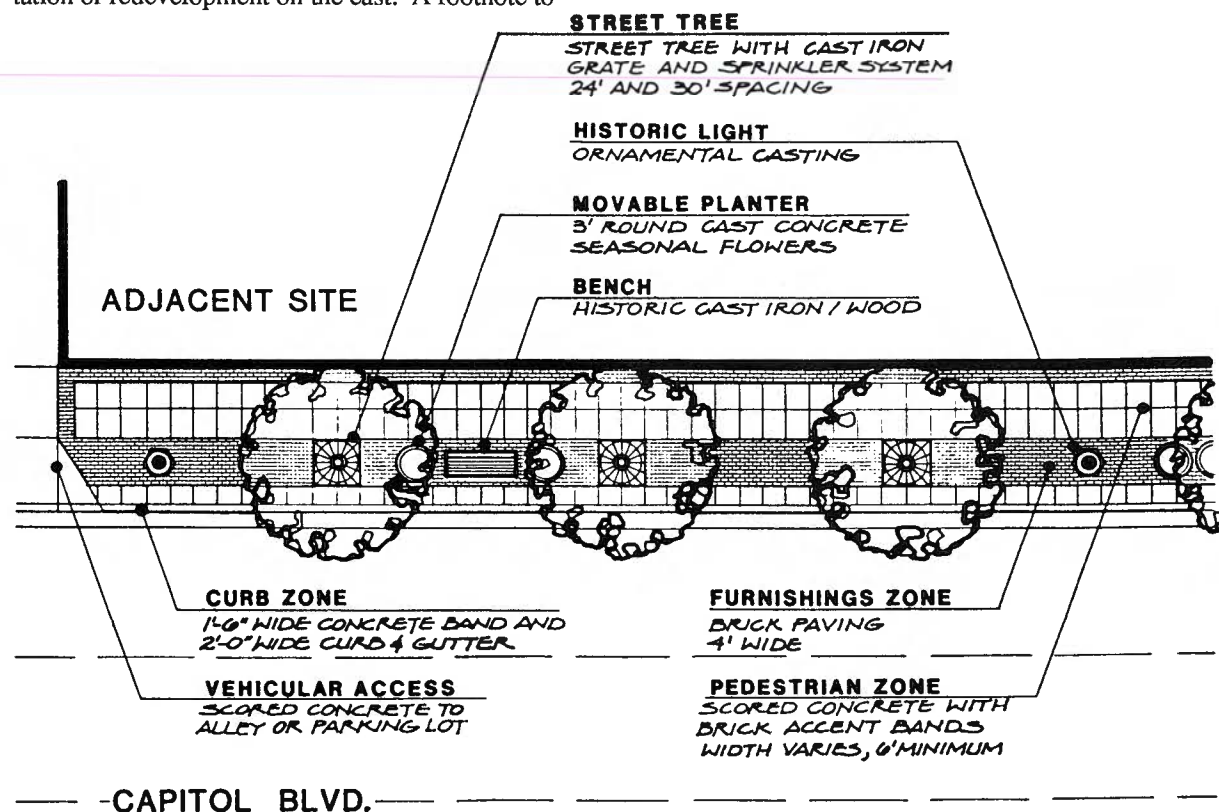


Figure 64. Streetscape prototype from Front Street to the Boise River. The adjacent private landscaped setback area could include an expanded sidewalk surface, plaza, or street wall to screen parking lots. An additional row of trees can also be planted on private property to form a double canopy over the sidewalk.

### 8TH STREET MARKETPLACE

A one-half block wide area, located behind the 8th Street Marketplace specialty shopping area and covering a two-block section that faces Capitol Boulevard, has the opportunity to be redesigned to better serve this historic district. An expanded Broad Street entrance is encouraged as well as compatible new development and a well landscaped park-like parking area. Future design work is also needed to help reduce the impact of Myrtle Street traffic on the historic district.

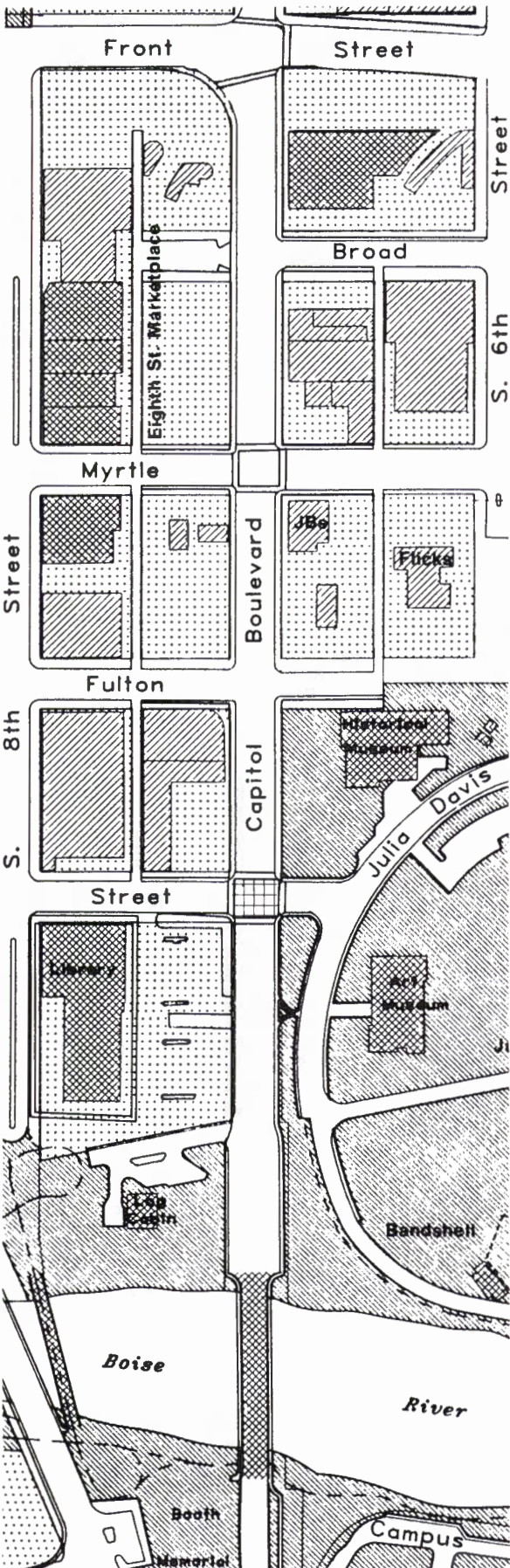


Figure 66. Front Street to Boise River area.

### JULIA DAVIS PARK

Long range planning is needed for the property north of Julia Davis Park and south of Myrtle Street. Efforts should be made to safeguard portions of this property through park purchases. Future opportunities will present themselves to develop portions of this area as part of the city's growing cultural center. While this area is in transition, additional park land should be acquired and held for future cultural uses (i.e. the proposed Idaho Mining Museum) that would add to the special uses that have formed there. These include the expanding facilities of the Boise Art Museum, the Idaho Historical Society, the Boise Public Library, and the Discovery Center.

The Capitol-Battery intersection is one of the boulevard's primary activity nodes and calls for special treatment. The intersection is paved with a bomanite brick surface accentuating the crosswalks. Further landscaping and demarcation are called for at all four corners.

Minor improvements proposed for Julia Davis Park include a specially designed and landscaped "Julia Davis Park" sign where the Battery Street entrance dead ends into the berm in the park. Other park projects include a new streetscape along the boulevard, including a wider sidewalk surface.



Figure 67. As part of Julia Davis Park a new 12-foot wide sidewalk/streetscape area would be built along the boulevard featuring sweetgum trees and historic lights.

### IDAHO FORESTRY BUILDING

The rustic log building located on the river is one of the boulevard's most indigenous, having been built in 1940 of Idaho wood products to help celebrate fifty years of Idaho statehood. The building's history and surrounding park features are described on page 10.

This two-floor, 4000-square foot building, under lease by the State of Idaho and owned by Boise City, has the opportunity to house a more active use that will complement the area's growing cultural center and greenbelt. This could be accomplished through the state or by returning the building to the city. A number of options for the building exist, ranging from a visitor's center, forestry museum, or other public or city use.

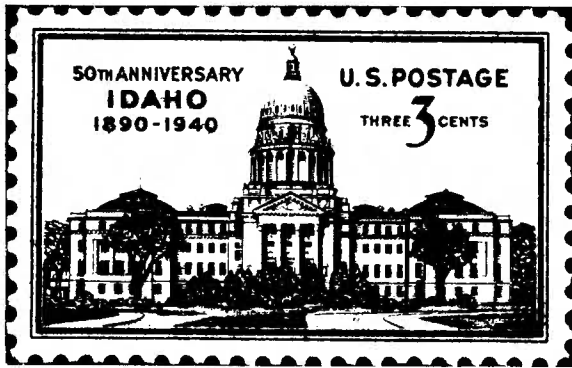


Figure 68. Idaho's Forestry Building, built in honor of Idaho's fiftieth birthday, was constructed the same year as the issuance of this 3-cent stamp.

A visitor's center facility is in great need to serve the City of Boise and the State of Idaho. With the building's natural log cabin charm and surrounding park and river setting it makes a favorable impression on visitors. Stocked with maps, brochures, directories, and a wide selection of travel literature, the center's helpful personnel would be able to respond to specific needs and present a warm and friendly atmosphere to the city's visitors.

Although the site's parking is limited and its approach would have to be adequately signed, the handsome facility and park grounds have the ability to function well, providing tourist information and a sense of Boise and Idaho to the traveler.

Curb side parking is available for vehicles as well as for larger RV's or vehicles with trailers along a wider section of Capitol just north of the bridge. Additional vehicle parking is located by turning left onto Girard Street (named in honor of the first State Forester, Franklin Girard) where seven parking spaces are located in a turn-around. A few extra spaces can also be designed into this narrow street. Access is also available off 9th Street where the Tourist Rest Stop parking area provides seven spaces, reaching the visitor's center by the 8th Street Bridge. It may also be possible to improve 9th Street access to parking for the site on the north side of the river. To assist the traveling public in locating the facility, "Tourist Information" signs (white legend on a blue background) would be installed.

One of the best features of the location is its park setting that includes the natural beauty of the Boise River. The site would be further enhanced by following the original landscaping plans and planting only shrubs native to Idaho, such as sumac, syringa, aspen, wild honeysuckle, and wild rose. Over the years, a prize display of native Idaho timber species has been planted and more would be added. The site is a major hub of the greenbelt system and includes the 8th Street Bridge (converted for pedestrian and bicycle use) which connects to the Tourist Rest Stop/Booth Memorial Park south of the river. Two memorials exist on the site, including the 1864 McClellan Ferry crossing and the 1931 Capitol Boulevard Bridge. An interpretive plaque proposed for the area would tell the river's history, including its crossings from the Oregon Trail, ferry, and four bridges.

Another proposal is to convert the log building into a forestry museum. Representing one of the largest industries in the state, this particular building well displays Idaho's wood products. Its exterior logs are Idaho Engelman spruce, and the interior of each room has its own special wood pattern with walls, ceilings, and floors covered with an individual native timber species. The forestry museum would provide an educational experience about the state's timber history and the importance of this natural resource. The museum would become part of the area's growing cultural center, including the library and art and historical museums. Funding and operations would come from the wood product's industry.

As the site is still in transition, future refinements to the park by the Boise Parks Department would be appropriate. One project under consideration is extending the greenbelt path under the Capitol Bridge on the north side, connecting to Julia Davis Park. This and other park improvements to the site will depend on the site's growing use and availability of funds.

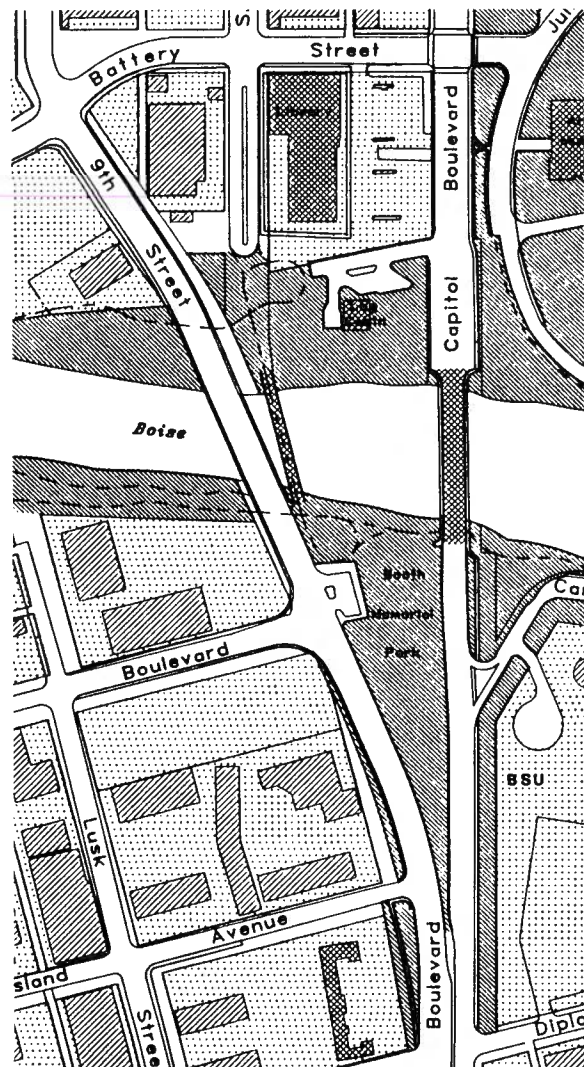


Figure 69. Vicinity Map.

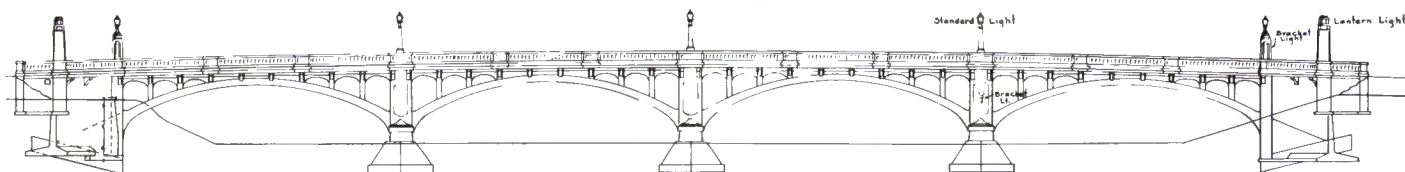


Figure 70. Capitol Boulevard Memorial Bridge. Elevation looking upstream.

#### CAPITOL BOULEVARD MEMORIAL BRIDGE

Along with the Capitol and depot the Capitol Boulevard Memorial Bridge is one of the boulevard's most significant features. Opened in 1931 with great fanfare, the bridge is a major part of the grand approach of the boulevard. Built by Morrison Knudsen Company and dedicated to the pioneers of the Old Oregon Trail, the bridge is constructed of reinforced concrete with four graceful arches spanning the Boise River. (See bridge history on page 7.) With its concrete arches, decorative pylons, and handsome balustrades this 301-foot bridge is an engineering art form. The Idaho Bridge Inventory, prepared by the National Park Service for the Idaho Transportation Department (ITD), states that "the Capitol Boulevard Bridge is of exceptional significance as one of the finest examples of concrete bridge architecture in the State of Idaho. The structure is outstanding for its beauty and unusual artistic features. Its prominent location as an approach to the City of Boise further emphasizes its symbolic significance."

Due to its significance, a rehabilitation program is recommended for the bridge. To understand the bridge's rehabilitation one has to understand its construction and various deficiencies and strengths. Possessing great aesthetic qualities, the bridge is an open-spandrel design with parallel rib arches in which the roadway lies on top of the arches. Formed of reinforced concrete and supplemented with steel the bridge has a 40-foot roadway, carrying four 10-foot wide lanes of traffic and having a cantilevered six-foot wide sidewalk on each side. The bridge has a 60-ton capacity with its approaches being a straight alignment. Bridge engineers Wendell Higgins and Joe Keller studied the bridge's condition as part of this report and have identified needed repairs to prolong the structure's life. The original design is capable of modern loads and can still serve the utilitarian needs of transportation. The bridge's long range traffic volumes will decrease slightly due to an improved arterial network and less semi-truck traffic will occur on the bridge as nearby trucking firms relocate over time due to changing land uses and increased downtown traffic congestion. The structural stability of the bridge's arches is good, and capable of carrying modern weight, but the decking has had problems cracking away from the structure. In 1987 the decking was chipped down and the rebar replaced, providing a new eight-year life span for the deck. Problems occur on the east and west handrails where the concrete is deteriorating and rebar is showing in some instances. Other problems include a few small cracks in the abutments. The bridge is inspected annually by the ITD and the Ada County Highway District and has a current sufficiency rating of 61.8.

It is recommended that all scored and chipped areas in the bottom of the deck and in arches, railings, and parapets be patched and the bridge cleaned. The bridge's decorative lights would also be repaired, including replacement of the missing lights on the arched piers. The original plans for the bridge are available from the ITD.

Rehabilitation funds for the bridge will come from both private and public sources. Funds have been solicited from Boise City Celebrations to reactivate the pylon lights and clean and repair the bridge's decorative mosaics and brass plaques. The builder of the bridge, Morrison-Knudson Company, could also assist with the bridge's rehabilitation as a centennial project. Public funds via the Federal government are made available to the ITD and, in turn, through the Ada County Highway District to be utilized to rehabilitate county bridges. Funding through the 1987 Surface Transportation and Uniform Relocation Assistance Act, carried out by the Federal Highway Administration, can be utilized for bridge rehabilitation. This act established that it "be in the public

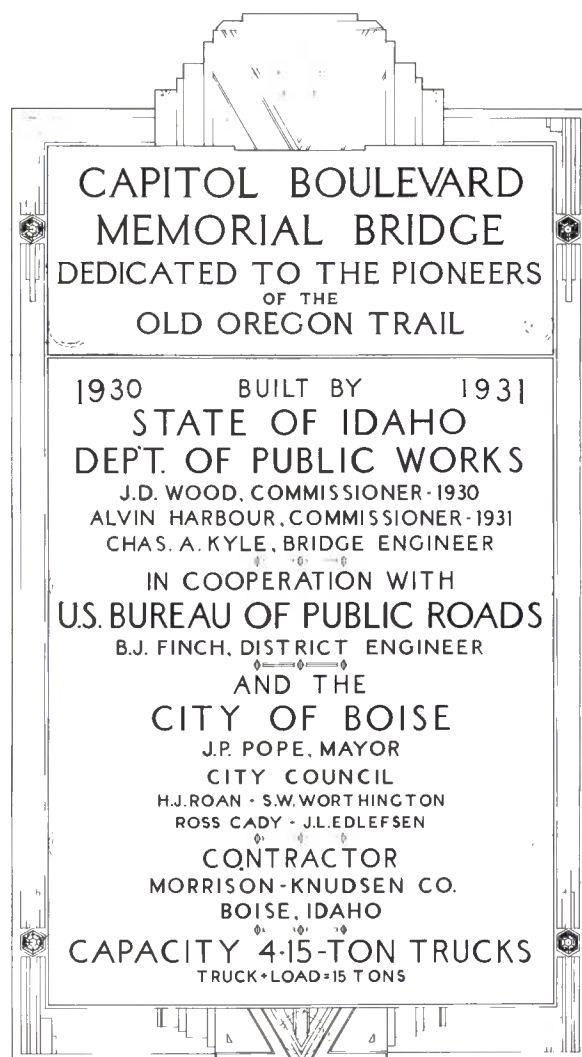
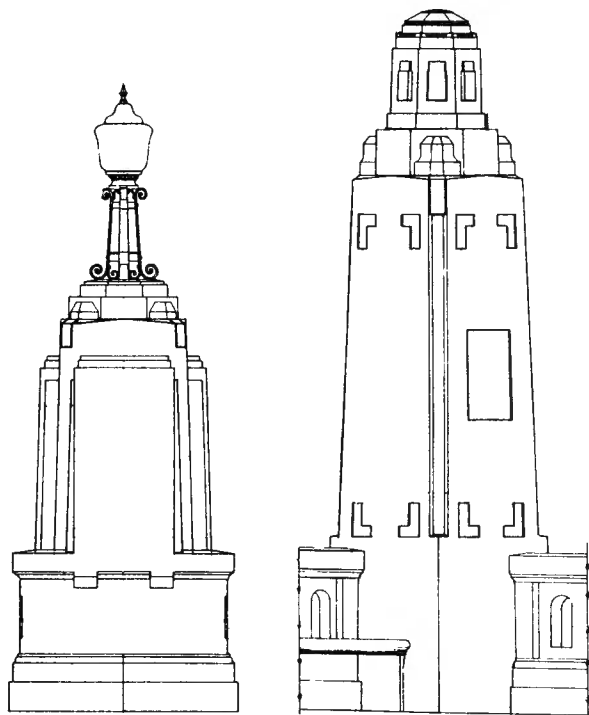


Figure 71. Other brass plaques and inlaid mosaic tiles include scenes of pioneer wagons crossing the river.



**Figure 72.** A pair of these decorative cement pylons are located at each end of the bridge.

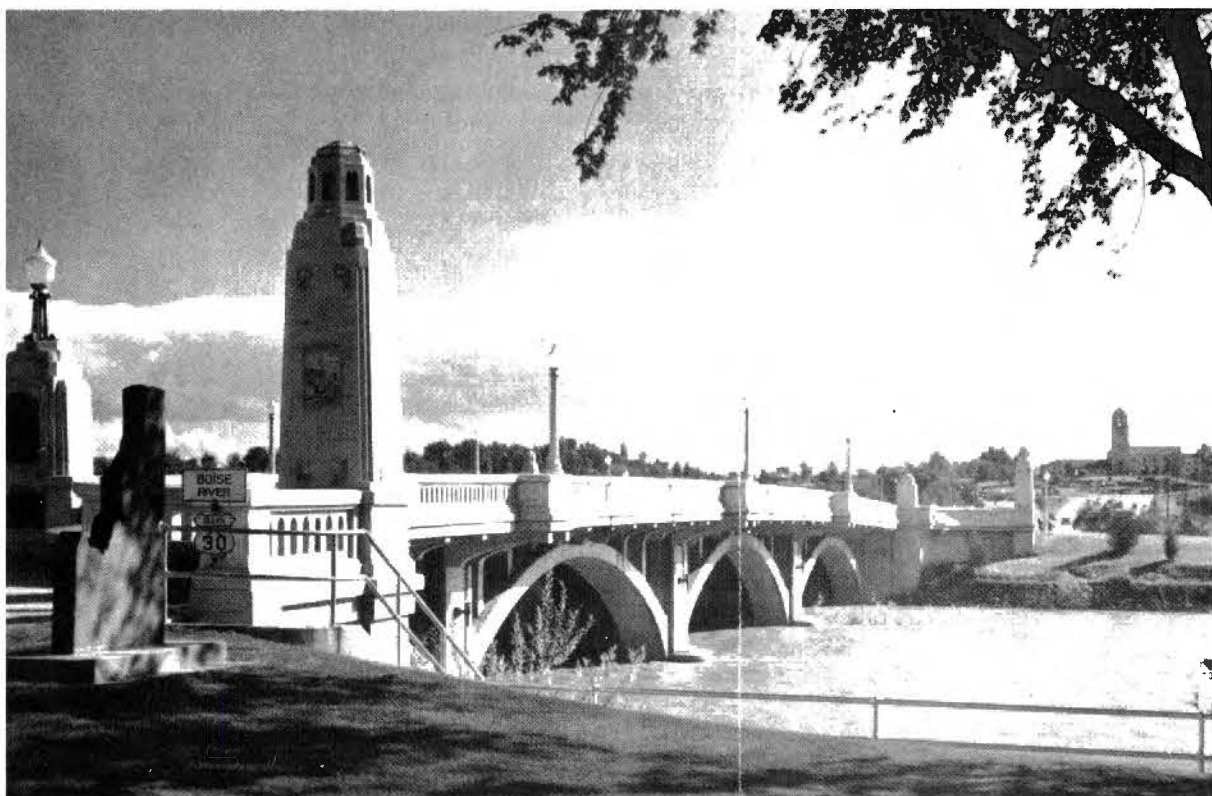
interest to encourage the rehabilitation, reuse and preservation of bridges significant in American history, architecture and culture. Historic bridges are important links to our past, serve as safe and vital transportation routes in the present, and can represent significant resources in the future.” The costs of rehabilitating the bridge would be less than the cost to demolish it and construct a new bridge. This is not to mention the loss it would represent—if demolished—as an important development in structural technology and bridge design. To further its future and assist funding opportunities it is recommended that the bridge be placed on the National Register of Historic Places.

Another component of the plan is the installation of an interpretive plaque on the 8th Street Bridge that reviews the progress of engineering technology in crossing the Boise River at this point. Text, sketches, and photographs would tell the story of the McClellan Ferry crossing that started in 1864, later being replaced by an early truss bridge that, in turn, was replaced by the existing 8th Street Bridge. (See Page 8.) The cement arched Capitol Boulevard Bridge further represents the dramatic advances in bridge technology as does the new 9th Street Bridge. The plaque would be installed by the Consulting Engineers of Idaho and the Idaho Society of Professional Engineers in cooperation with the Boise City Parks Department.

#### BOOTH MEMORIAL PARK/TOURIST REST STOP

Discussed on page 20, this small triangular greenbelt park has been recently relandscaped with new trees and turf and the greenbelt path expanded to connect to the historic 8th Street Bridge and under the new 9th Street Bridge. To complete this park, a pedestrian path is needed to connect the south end of the west sidewalk of the Capitol Bridge to the greenbelt. Also called for is a greenbelt map to direct users to the various routes connecting to the site. The park would be rededicated with a sign and plaque.

As the park site is one of the most visible from both Capitol Boulevard and South 9th Street it is proposed that a major public sculpture be purchased and placed in the center of this capital city park at a future date. A monumental structure, unique in design, is envisioned.



**Figure 73.** An early photo of the bridge looking south. Notice McClellan Ferry Monument on the north side and former historic light pole on the south side.

## BOISE STATE UNIVERSITY

Comprising the longest uninterrupted segment of the boulevard is the campus of Boise State University (B.S.U.). Stretching from Campus Drive just south of the Boise River to Diploma Street, the boulevard frontage is nearly 800 feet in length. This area has a six-foot wide sidewalk abutting the curb as well as a flat 80-foot wide or wider grassy area dotted by trees and shrubs. Within this setting are two B.S.U. sign structures—a brick and wrought iron sign and an electronic reader board.

As part of the boulevard's cultural and educational center, Boise State University has grown rapidly to become the state's largest academic institution. (See page 26.) With its numerous facilities, the university creates a continuing synergy of activity for the boulevard, including cultural and sports events of community interest as well as a wide spectrum of academic endeavors. Growing faster than any other of the city's institutions, B.S.U.'s divergent components make it one of the boulevard's most important resources.

Collegiate building at the Capitol Boulevard end of the campus has been limited and no major building or feature defines the university's presence. The 1970, seven-story, twin Towers Dormitory (located near the Boise River some 200 feet from Capitol Boulevard) helps maintain the spaciousness of the boulevard and wide views of the river as one travels down Capitol. Just behind the Towers is the 1700-seat Morrison Center for the Performing Arts. This hall is a major

component of the boulevard's cultural center function and helps set the tone for future west side campus development.

A major Fine Arts Building, which is under consideration for development along Capitol Boulevard, would complement this growing cultural center and create an imposing image for the university. Such a building needs to be of superlative design with its placement adding to the symmetrical alignment of the boulevard. Set back from the boulevard within a landscaped setting the building would become the focal point of the west end of the campus.

Another opportunity for B.S.U. is to create a gateway into the campus along Capitol Boulevard. This could be accomplished through a narrow quadrangle space that would penetrate into the west end of the campus to the Micron Center. The Towers, Morrison Center, and Education buildings are aligned on the north side of the proposed quadrangle and new buildings and parking would be built along the south side.

Treatment of the grassy edge along Capitol Boulevard calls for moving the sidewalk away from the curb and building a new 12-foot wide sidewalk and bike path set within this landscaped space. A row of red oak trees planted 50-foot on center are to be planted along the boulevard to reinforce its linear character. These trees, which are similar to ones recently planted in Booth Memorial Park directly across the boulevard, are part of a Centennial tree planting project planned for the boulevard and funded



*Figure 74..* 1988 aerial view of the southern portion of Capitol Boulevard. Ann Morrison Park at top, B.S.U. lower right, and residential neighborhoods on the left. Photo: C. Bowers.

by Boise Cascade Company. The existing trees set within this swath of grass, which include linden, ginkgo, honey locust, red oak, spruce, and pine, would be maintained. Another important link to the boulevard's pedestrian and bicycle system would be created by improving the greenbelt approach connecting this new sidewalk to the bridge underpass.

This plan also recommends enhancing University Drive as the primary vehicular entrance to B.S.U. by incorporating a tree lined drive with medians and distinctive light fixtures. University Drive, in conjunction with Ann Morrison Park Drive, would take on a more formal design creating an east-west radial alignment off of Capitol Boulevard. This approach of radial arms off the boulevard is similar to concepts presented in the 1924 Boulevard Plan. (See page 14.) One feature that helps define University Drive is that its alignment terminates on the bell tower of the Administration Building.

Part of this entrance improvement includes purchasing the current Blimpie's lot. This key corner property is crucial to setting the image for the university's entrance and allowing the appropriate approach to the proposed Fine Arts Building. A new B.S.U. sign is also needed at this location. Likewise, the B.S.U. electronic reader board requires a new structure that is more compatible with a college campus. This electronic message board plays an important role in informing the public of various events and activities taking place on campus. Keeping with the boulevard signing policy the reader board would not be utilized for private advertising. (See page 33.)

B.S.U. student housing built on Boise Avenue just off Capitol Boulevard is well located to serve the campus and support the existing neighborhoods to the east and south. Over time, additional student housing should be located in this area, including at the trailer park area below depot hill.

#### **CAPITOL/UNIVERSITY/ANN MORRISON PARK DRIVE INTERSECTION**

South of the Boise River, this major intersection provides the only east-west vehicle/pedestrian crossing of Capitol Boulevard. Complicated by the fact that Boise Avenue makes it a five-sided intersection, this point serves as the primary vehicular entrance to Boise State University, as well as serving Ann Morrison Park and the adjacent commercial area just east of the park.

As discussed on page 16, this large intersection faces congestion as peak southbound traffic experiences delays due to long signalization sequences and the influx of traffic caused by the activity of B.S.U.

Current plans call for through traffic to be routed onto Boise Avenue and B.S.U.-destined traffic to utilize University Drive. Traffic circulation solutions for this intersection require further study and are

beyond the scope of this report. In the interim, better signal synchronization may help.

This intersection is also a major pedestrian node and requires improvements to make it easier and safer to cross. The crosswalks should provide safe use for this intersection, especially as the number of pedestrians and bicyclists continues to increase at this point with its surrounding restaurants, motels, park, and university use. Future improvements should include installing a brick or special paving material to better define the crosswalks. As a temporary measure, specially painted or thermoplastic zebra stripes should be used. Bollards should also be installed at the corner curbs and possibly in the median to provide additional safety for the pedestrian.

Another improvement to the intersection would be to redesign the large overhead signal structure that spans the boulevard since it is overly massive, negatively impacting the visual qualities of the street. The narrow, cement median that connects to Booth Memorial Park from this point also should be surfaced with a red brick or bomanite similar to the median design going up depot hill.

Landscaping improvements on the four private parcels facing this intersection are also needed. Likewise, a small public space in front of University Inn's parking lot should be landscaped.

#### **ANN MORRISON PARK**

Another major feature of the boulevard is Ann Morrison Park. This 153-acre park provides a beautiful expanse of varied landscapes, including ponds, picnic areas, sports fields, gardens, and trails. (See park history on page 20.) The connection of the park to the boulevard is excellent along the greenbelt, which connects under the Capitol Boulevard, 9th Street, and 8th Street bridges to B.S.U. However, the park's eastern links along Ann Morrison Park Drive and Royal Boulevard do not present the stature or appearance appropriate for the entrance to Boise's largest developed park. This entrance is particularly uninviting to pedestrians as no sidewalks exist. To resolve this, it is proposed that Ann Morrison Park Drive be redesigned and landscaped and given a formal park entrance. Similar to improvements recommended for University Drive, Ann Morrison Park Drive would become a major radial arm of Capitol Boulevard. A new park entrance plan needs to be prepared that will include a design of street trees, a median, sidewalks, and a new main park sign. Likewise, Royal Boulevard off of South 9th Street would be lined with trees, new sidewalks, and improved directional signs. The improvements would be accomplished under the direction of the Boise City Parks Department through both adjacent private development and ongoing funding efforts for the boulevard.

## BOISE CITY PARK SUBDIVISION

Platted as the Boise City Park and Fifer subdivisions, the large, flat area just south of the Boise River between Ann Morrison Park and Capitol Boulevard presents a number of development opportunities. Accessed by the new, one-way southbound 9th Street or Ann Morrison Park Drive this area's development has been in transition. Improvements over the years have included the expansion of educational use from the ITT Technical Institute and residential growth at the Sherwood Apartments and Victors Motor Inn. Victors has the opportunity to make an important improvement to its front door by relandscaping its parking area along South 9th Street. This public land is the former South 8th Street right-of-way and may be purchased from the Ada County Highway District. This purchase should be conditioned on the installation of an appropriate landscaping treatment that is in keeping with the grandeur of Capitol Boulevard. Presently, the adjacent Boulevard Motel is working on making its multi-gabled 1940's design into a first-class motel with decks and gardens added to the rear.

To further the development of this area, a future master plan needs to be prepared. Consideration should be given to expanding and strengthening the area's residential character and complimenting it with additional institutions, offices, or motel/hotel development. (See Figure 74.) A possible long-shot option includes developing the area for an exposition, such as a Year 2000 Technology Exposition.

## DEPOT HILL

Few modifications are necessary for depot hill, which was rebuilt in 1985 and included a ten-foot wide sidewalk separated from the street by an eight-foot landscaped area on the east side of the boulevard. This separated sidewalk covers about three-quarters of depot hill and needs to be completed and set back from the curb all the way to University Drive, including bicycle curb cuts at Boise Avenue.

Ginkgo trees funded by the Boise Cascade Corporation are to be planted on both the east and west sides of the hill to reinforce the linear pattern and help screen out adjacent uses, such as the trailer park and Outdoor Advertising Company's storage area.

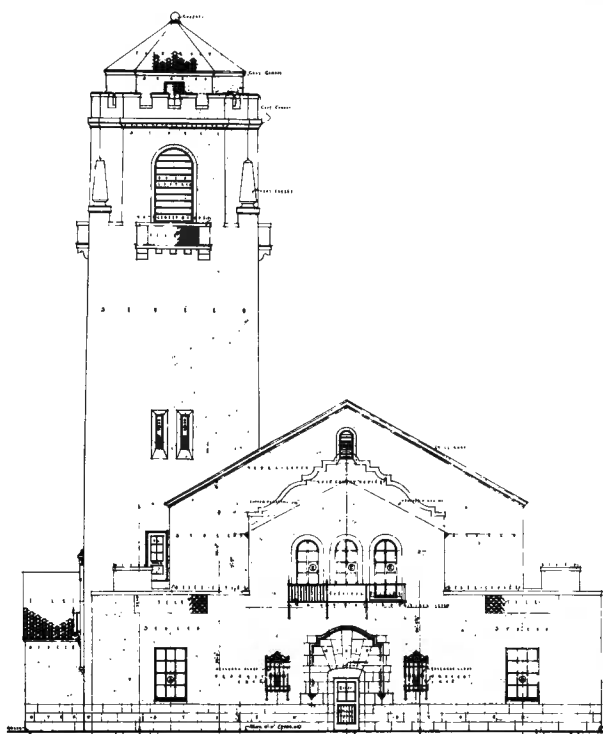
Another project involves modifying the large freeway style guide signs over the southbound lanes to a less obstructive design.

Due to the limited public right-of-way in front of the Totem Building, the owners are encouraged to plant a couple of Ginkgoes along the boulevard property line. Another opportunity is to clean up and landscape the Budd Canal and small pool that lies between the Totem Building and Elmers Restaurant. This small canal, which flows into Ann Morrison Park, can be enjoyed as a part of an outdoor picnic area by Papa Joes Restaurant. Overhead power lines in this area also need to be placed underground.

Other improvements recommended for the depot hill are installing low maintenance landscaping on the bench slopes and designating Crescent Rim Drive as a scenic drive due to its panorama.



Figure 75. Depot hill looking north. Notice sidewalk setback on the east side of the hill.



**Figure 76.** West elevation of stucco, sandstone, and tile depot.

#### UNION PACIFIC DEPOT AND PLATT GARDENS

The striking Union Pacific Depot and lush Platt Gardens are one of the boulevard's most memorable experiences. Overlooking the city, the depot's fine architectural characteristics are set off by its majestic bell tower and reception room, and surrounded by gardens with a splendid rock grotto and ponds. (See pages 6, 13, 17, 20, and 22.)

The building and gardens are in good condition, generally, and function well as a city park, Amtrak station, and UP offices. In honor of its importance to Capitol Boulevard and in commemoration of Idaho's Centennial, a number of building and ground improvements are recommended. While maintaining its primary Amtrak and railroad function, the Union Pacific Railroad Company should consider allowing a multi-use concept for the building. A cultural or museum function would be most appropriate as would a quality restaurant. Such a use would also raise additional revenue to support the building. Improvements call for repairing the tower's chimes and reactivating its nighttime illumination, which, until recently, basked the tower in light. Other repairs include painting, general maintenance, and resetting some of the platform's brick pavers which have held up well with over sixty years of use.



**Figure 77.** Depot and gardens in 1927.

Garden improvements call for resetting a number of the sandstone blocks that make up the park's meandering paths and stairs and repairing the grotto's stalactites. The ponds need to be cleaned and the shrubbery that surrounds the grotto and ponds, which is overgrown, needs to be trimmed back.

A new main "Platt Gardens" sign (wood with sandstone base) is recommended along with an interpretive sign that would identify the history and special features of the depot and gardens.

These improvements would be undertaken by the Union Pacific Railroad Company and the Boise Parks system.



**Figure 78.** Depot bells.

#### VISTA AVENUE/FEDERAL WAY CONNECTIONS

These two arterials connect Capitol Boulevard to the Boise Air Terminal and Interstate 84 and require both traffic and aesthetic improvements as primary Boise City entryways. Plans are currently underway by the Ada County Highway District and the Vista Neighborhood Association to widen and beautify portions of Vista Avenue by 1991. Between Malad and Overland streets the avenue would be widened to four lanes plus a middle turning lane, creating a uniform design for the avenue. Funding efforts are underway to landscape nine medians with street trees 25 feet on center and install Boise historic light fixtures in this area. The Boise City Parks Department would maintain the medians. Such treatment and additional landscaping of Vista Avenue is a major component of enhancing Capitol Boulevard and requires the support of the boulevard's various entities.

Another improvement that will assist the boulevard is to provide guide signs at the Vista Avenue exit on I-84 for the zoo, museums, and state Capitol.

Federal Way has also been identified for future improvements. Upcoming studies need to identify appropriate amenities that will take advantage of Federal Way's bench view as well as its traffic needs. Development along Federal Way is minimal at present and can be guided to enhance this scenic entryway and protect the utility of the arterial.

## IMPLEMENTATION

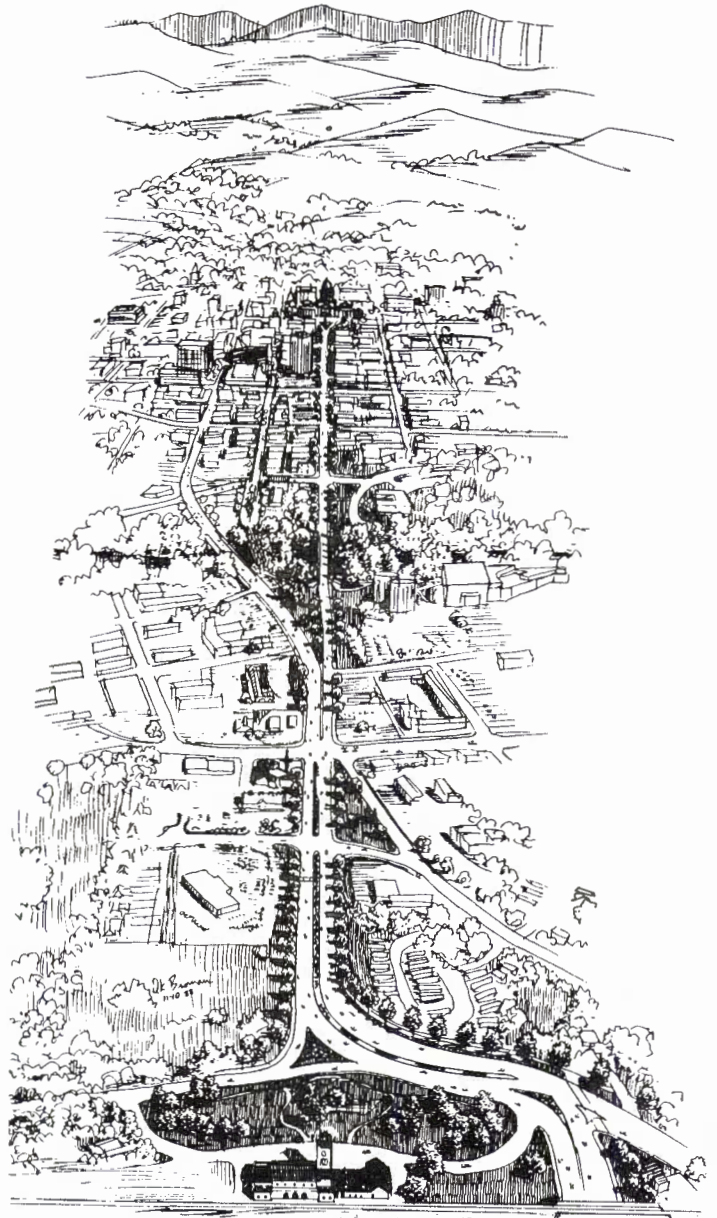
Striving to achieve a new urban grandeur for Capitol Boulevard, the plan serves as the impetus for making the boulevard a fitting centennial tribute to the State of Idaho and City of Boise. The plan is intended to be both economically feasible and aesthetically responsive to the ceremonial character of the boulevard. The boulevard's environment is to be developed as a single unified element rather than a series of piecemeal policies or efforts. The plan will be accomplished through a cooperative effort that combines public improvements with private development.

The complete restoration of Capitol Boulevard is a long-term process requiring a number of public capital projects and adjacent private redevelopment. The first step is the city's official adoption of the plan and its standards described herein. As outlined in the management element on page 35, the key public agencies implementing the plan include the Ada County Highway District (ACHD), the Boise City Parks Department, and the Public Works Department. Likewise, a commitment is required from the Boise City Planning and Development Department, the Planning and Zoning Commission, the Design Review Committee and the Historic Preservation Commission to assure that the plan will be carried out.

Boulevard improvements will occur as the city and county upgrade Capitol Boulevard's public spaces as a part of special projects or through the normal cycle of reconstruction in the public right-of-way. Examples include the Capital City Development Corporation's upcoming work on the boulevard's streetscape, the park department's improvements of Capitol Park, the ACHD's future intersection and sign work, and upgrading of light standards by the Public Works Department. Boulevard improvements would also be realized through reconstruction necessitated by private redevelopment. Numerous development opportunities are available and will result in future boulevard refinements.

Funding to carry out the Capitol Boulevard plan will come from a blend of both public and private sources. Funds for public works projects are normally the province of city, state, and federal governments, and the boulevard's revitalization and maintenance would continue to seek and utilize those funds. Another option for raising additional public funding would be for the citizens of Boise to support a bond issue. Such bond issues to support improvements for Capitol Boulevard were passed in 1919 and 1924.

In honor of Idaho's centennial, funds have been solicited from private interests and Boise Centennial Celebrations to finance special projects along the boulevard. Initiating this effort was the Boise Cascade Corporation, which donated \$40,000 to plant 100 trees along the boulevard. Likewise, Albertson's, Inc. has donated \$55,000 to help honor Idaho's 44 counties by mounting county shields on Capitol's



**Figure 79.** Birds-eye view of the 100 trees donated by Boise Cascade Corporation to be planted along Capitol Boulevard for the 1990 Centennial.

light fixtures. West One Bank and Idaho Power Company have contributed to installing additional historic lights. First Security Bank is also assisting with the bridge relighting project. Other projects include park improvements, streetscape and lighting work, bridge repair, and boulevard pageantry.

Additional funding sources include federal Community Development Block Grants and/or federal park and transportation funds. The CCDC has the opportunity to invest in the streetscape on both sides of Capitol within their project area. Another option is to expand the downtown's tax increment financing district to include the Capitol Boulevard Special District. Additional property taxes raised in this area would then be able to be reinvested in the district's various projects for a certain period of time. With Capitol Boulevard being of state importance, funds will be sought from the State of Idaho through the legislature, Idaho Department of Transportation, and the state Centennial's Idaho Heritage Trust Fund. Other funds will come from the boulevard's endowment and special projects, such as a Statue of Liberty

type fund raising effort and returns from selling posters and cards that represent the boulevard's artistic and scenic qualities.

Ongoing management will come from public agencies, a Business Improvement District (BID) and a non-profit board overseeing a Capitol Boulevard endowment. A Capitol Boulevard Business Improvement District would become a sub-area of the existing downtown BID, allowing those businesses along the boulevard to assess themselves a small fee to work cooperatively in promoting the district. (See district boundary, page 2.) Projects could include helping fund special events, promotion activities, beautification, and maintenance.

To perpetuate and foster the care and maintenance of Capitol Boulevard, it is proposed that an endowment be established. Private contributions to the endowment will provide the critical difference between maintaining the boulevard and managing it as a first-class facility. The endowment fund would rely on the help of individual, corporate, and foundation donors. As envisioned, annual interest from the million and one-half-dollar endowment would equal \$120,000 for yearly improvements when invested in conservative, long-term yields of approximately 8% interest. The annual interest funds would be overseen by the Friends of Capitol Boulevard in close coordination with the Boise Parks Department.

The Friends of Capitol Boulevard will be incorporated as a non-profit tax exempt corporation (503-C3) and run by a Board of Directors, which would represent the existing American Institute of Archi-

ects, who developed this plan, and a broad segment of the community. Efforts are underway to initiate this group and locate proponents of the organization and its endowment drive. Promotions, walking tours, and events would be held to raise money and increase awareness of the boulevard as a special place. The effort would be a grass roots initiative looking for support from all levels.

A rallying of efforts in time for Idaho's 1990 Centennial will allow the boulevard to become the pride of Boise and Idaho and help preserve its ceremonial qualities for future generations.

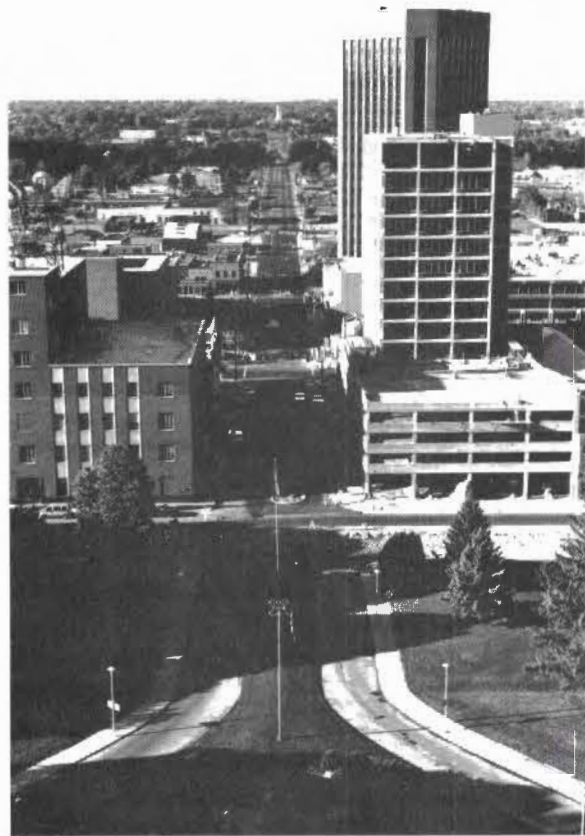


Figure 81. 1988 view from Capitol dome looking south.

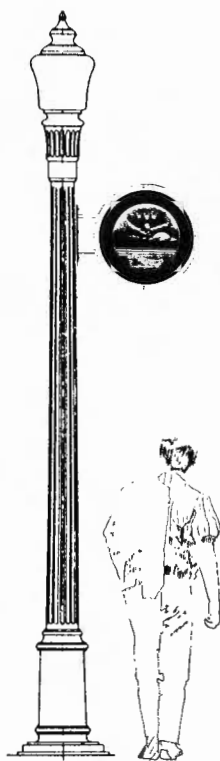


Figure 80. Centennial pageantry includes mounting county seal shields on Boise's historic light standards located on Capitol Boulevard to honor Idaho's 44 counties.



Capitol Park - Centennial Celebration

Streetscape Prototype  
Boise historic light fixtures, maple trees, and brick pavers.

County Shields of Idaho

Basque Neighborhood Marketplace

Future Hotel and Continuing CBD Redevelopment

Development Opportunities  
8th Street Marketplace  
Additional development and landscaping

Streetscape Prototype

Julia Davis Park Museum Complex

Idaho Forestry Building Visitor Center

Capitol Boulevard Memorial Bridge Rehabilitation

Boise State University - Greenbelt Connections  
Fine Arts Building, landscaping, and new quadrangle space off of the boulevard.

Prepare Boise City Park Subdivision Plan

Improve Traffic Flow and Intersection Design

Grand Entrance to B.S.U. and Ann Morrison Park

Boulevard Tree Planting

Depot and Platt Gardens Restoration

Vista Avenue and Federal Way Connections

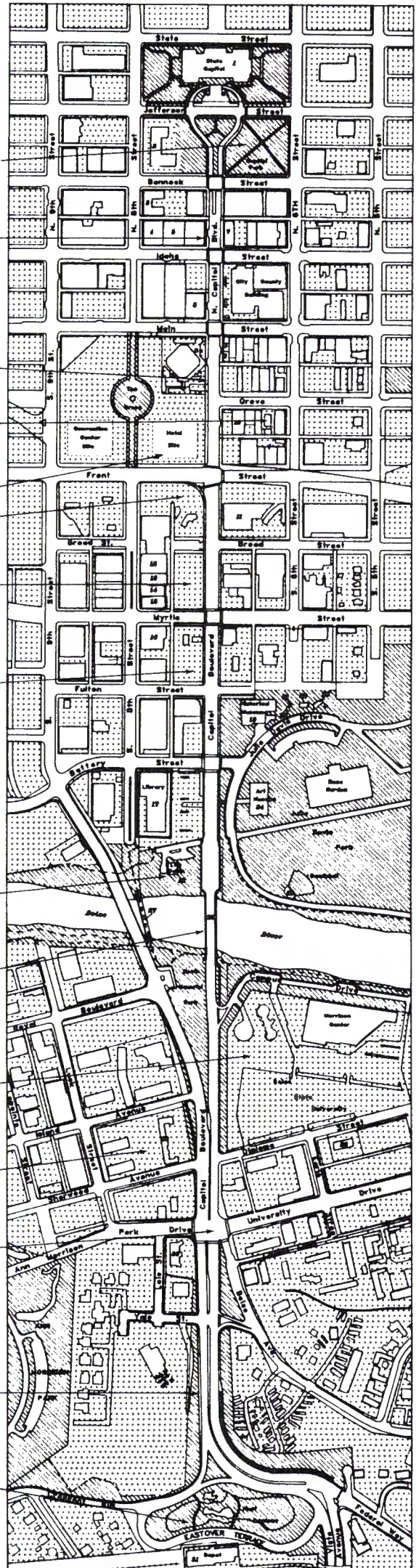
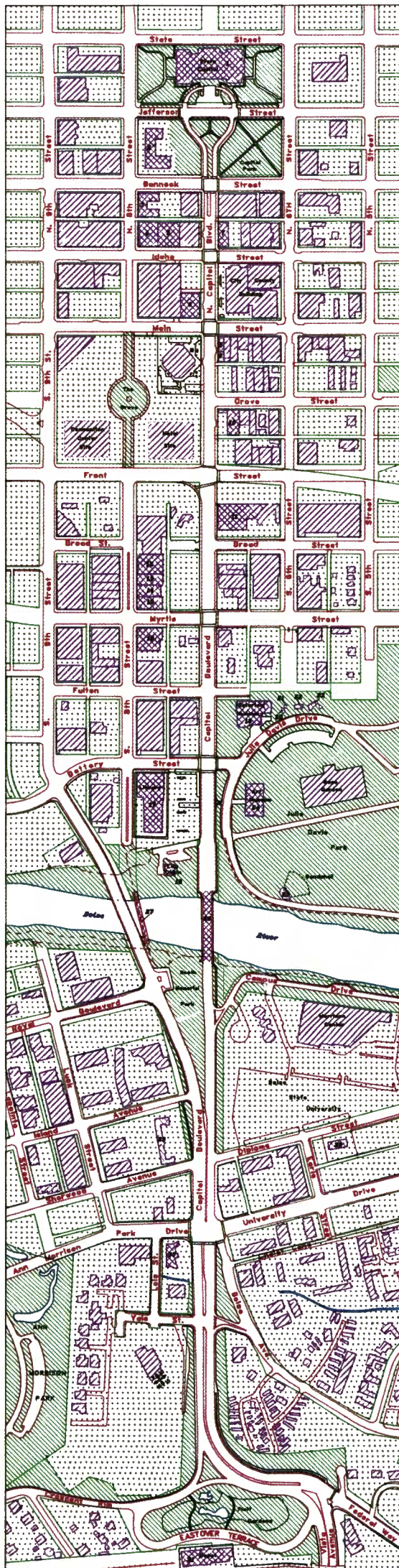


Figure 82. Capitol Boulevard Plan identifying project's components.

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Computer based map prepared for Capitol Boulevard Plan and Action Program.